

# 34th STREET SPECIFIC AREA PLAN

ADOPTED \_\_\_\_\_

*VERSION 1 (Revised 6/5/2019)*

# CREDITS

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## LEGAL ADVISORY ACKNOWLEDGEMENTS

Our team would like to acknowledge our legal team for their support, advice and guidance in preparing this plan.

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## VISION & GOALS

The Live, Work, Create District is a special place. As it takes shape it is a neighborhood that promotes creativity. There is something truly special about being able to make and create. Our 34th Street Specific Area Plan is designed to be very interactive, each use and design is carefully selected to allow for a experience by just taking a walk!

Food bonds a community; while you enjoy a coffee on 34th and Carr Street you can also watch an artist painting. I believe that food is one way to help support "place making". Along with the idea of an interactive street scape, is the goal of bringing an interactive Farmers Market to Garden City. This will be future indoor and outdoor space with the goal of spanning the entire length of 34th ( weekly and seasonal). At the Market you can enjoy local foods, buy fresh local produce, shop for unique art. A farmers market celebrates local

businesses and encourages community. Food is a wonderful way of uniting community.

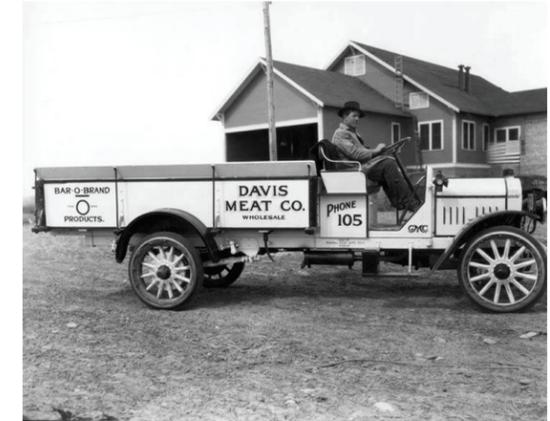
Our 34th Street Specific Area Plan is designed to allow Mixed Use buildings, this is important to us so the neighborhood can be a pedestrian focused environment where residents and community members can walk and explore.

One of the first ideas that was discussed in planning was creating a "plaza" an outdoor space that all can enjoy and the can make our community special. We designed the plaza to be very natural, very welcoming and pedestrian friendly. The goal for the plaza "The Eddy" is to tie all the elements together; in the plaza you will see art, culture,

historic monuments, industrial references, elements of the Garden, food, and community enjoying our wonderful community and provides river activated uses and design.

Incorporating art, History and Culture into our plan was a high priority. Our 34th Street Specific Area Plan will provide historical references on what uses or activities have taken place in a given location. In the Plaza we want to have monuments that provide a timeline to our evolution of Garden City.

We are pleased with our 34th Street Specific Area Plan and look forward to bringing it to the Garden City community. We've scored our project using the Idaho Smart Growth Score Card as you can see below.



**Smart Growth Neighborhood Development Scorecard**

Rate each criteria on a scale of 0 to 4. Give the development in question a zero if it does not meet the criteria in any way and a four if it meets the criteria perfectly.

This development meets the listed criteria

Not at All | Somewhat | Perfectly

0 | 1 | 2 | 3 | 4

Record Score

Land Use Criteria	Score
1 The project is inside city limits or will be annexed (4), is inside an area of city impact (2-3), is outside existing planning areas (0-1).	4
2 The project defines a neighborhood(s) that is roughly a ten minute walk from edge to edge (approx. 1/2 mile).	4
3 Buildings are zoned by compatibility of building type first, use second; e.g. single family/home office or apartment/office are compatible if building form is similar.	4
4 Street trees, sidewalks, front porches and front doors dominate streetscapes, not garage doors and driveways.	4
5 There are a variety of housing types and sizes that at least two income levels can afford.	4
6 Most lots are less than 70 feet wide. There is rear alley garage access.	4
7 There is an elementary school with pedestrian access within one mile of the neighborhood.	4
8 There is a variety of housing density and housing density is higher the closer you get to the neighborhood center.	4
9 Small green spaces and playgrounds are located within 1/4 mile walk of every residential unit.	4
10 Building setbacks are shallow, generally no more than one quarter the lot width, with a maximum of no more than 20'.	4
11 There is a neighborhood center with retail (best), office, a public meeting space, and/or a park or other green space within 1/2 mile of all residents (may/may not be part of project).	4
12 Commercial buildings front directly on the sidewalk with parking to the side or rear, and/or open spaces/parks are fronted by roadways rather than behind backyards.	4
13 On street parking is encouraged. Parking lots are generally located behind street walls and/or buildings with little street visibility.	4
14 The project works with the natural topography and minimizes grading. Most natural amenities are retained, or new amenities constructed.	4
15 The project approximates pre-development drainage conditions and reduce water pollution potential by using measures such as on-site biofiltration.	4
16 The buildings use sustainable, energy efficient materials, appliances and design.	4
17 Landscaping conserves water, preserves/uses native plants, preserves/replaces existing trees - especially specimen trees, and/or enhances the site with new trees.	4
<b>Land Use Criteria Subtotal</b>	<b>68</b>

Carry this subtotal to the other side (over)

**IDAHO Smart Growth**

*Neighborhood Development Scorecard continued*

This development meets the listed criteria

Not at All | Somewhat | Perfectly

0 | 1 | 2 | 3 | 4

Record score

Transportation Criteria	Score
18 Streets integrate all modes of transportation, with safe and comfortable sidewalks and pathways throughout. The project has transit access (or access is planned).	4
19 Streets are organized in a connected network internally and are connected to existing or planned adjacent streets. Blocks are short (<400')	4
20 Culdesacs are avoided except where absolutely necessary due to natural conditions.	4
21 Traffic calming measures such as curb bulb-outs are incorporated.	4
22 Roadways are relatively narrow (e.g. 29' from curb to curb for local residential streets) and parking is allowed on both sides of streets.	2
23 Sidewalks are 4-5' wide and detached, or >10' wide at the neighborhood center. 5-10' tree planter strips have shade trees planted an average of 30' on center.	3
24 Buildings front on to collectors. Street section design of collectors and arterials is sensitive to the surrounding land use and usable by all transportation modes.	4
25 There is a dry, dignified place to wait for transit in the neighborhood center.	0
<b>Transportation Criteria Subtotal</b>	<b>25</b>
<b>Land Use Criteria Subtotal from front</b>	<b>68</b>
<b>Grand Total (Land Use + Transportation Criteria)</b>	<b>93</b>

Now add up all of the scores and then add the subtotals to get a grand total. The highest possible score of 100 means the development meets smart growth principles 100%.

<b>80-100 pts. — Congratulations. This is an excellent smart growth neighborhood.</b> ★
<b>50-79 pts. — Good effort, look for small modifications that might increase the score.</b>
<b>25-49 pts. — Needs major improvements to meet smart growth principles.</b>
<b>0-24 pts. — This is not a smart growth development.</b>

For more information on how to use the Idaho Smart Growth Scorecard please contact us at: P.O. Box 374 Boise ID 83701 Phone (208) 333-8066 isg@idahosmartgrowth.org







### ILLUSTRATIVE PLAN

The Illustrative Plan is an overview that includes all of the participating properties and their proposed development. The intention of the 34th Street Specific Area Plan is to create a framework for a transit-supportive urban node with an active, pedestrian and bicycle oriented, mixed-use corridor at the heart of, and embodying the soul of, the Sural Mitchell Live Work Create District.

34th Street and W. Chinden is the entranceway to the mixed use/commercial corridor neighborhood, with building elements and entranceway art. The 34th Street corridor continues with mixed use and Live/Work/ Townhomes buildings and active open space plazas, and ends with the public plaza and river-activated uses at the plaza at the Boise River and Greenbelt.

Streets: The existing street grid will remain, with E. 34th Street as the primary connection from Chinden Boulevard to the Boise River Greenbelt. It will be improved from W. Chinden Boulevard to N. Carr Street as a shared vehicle/bike street (sharrow) where it then will become a 50' wide pedestrian/cycle dismount corridor opening up to a river-activated plaza, Boise River and Greenbelt. N Carr Street will be a similar sharrow and both streets will have wide attached sidewalks with pedestrian/cyclist/transit amenities – corner bulb-outs, benches, trees, planters, bike racks, transit stops, and public art. Clay Street and E 33rd, 35th Streets will have detached sidewalks, with similar amenities. NOTE: Street improvements will only occur at participating lots, though improvements are shown at non-participating lots.

Buildings: Buildings will be close to the sidewalk and face the streets with Live Work Create activities – main entrances, retail, restaurants, offices, Live/Work/Townhomes, patios, plazas, and connecting walkways to make a lively streetscape. Buildings facing the plaza will have river views and river-activated uses – main entrances, restaurants, retail, patios, decks, and balconies above.





**Plaza:** The plaza is the focal public area of the 34th Street Specific Area Plan drawing people to and from the Greenbelt into the LWC District, with river-activated uses and activities including retail, restaurants, and events for all ages/families/children/pets. The plaza will have seating, kiosks for farmers markets, planted and lawn areas, shade structures, play areas, equipment for supporting events, and surfing amenities.

**Parking:** Vehicular parking is screened from street view, located behind or behind/inside buildings accessed off of new drive aisles created

ILLUSTRATIVE PLAN

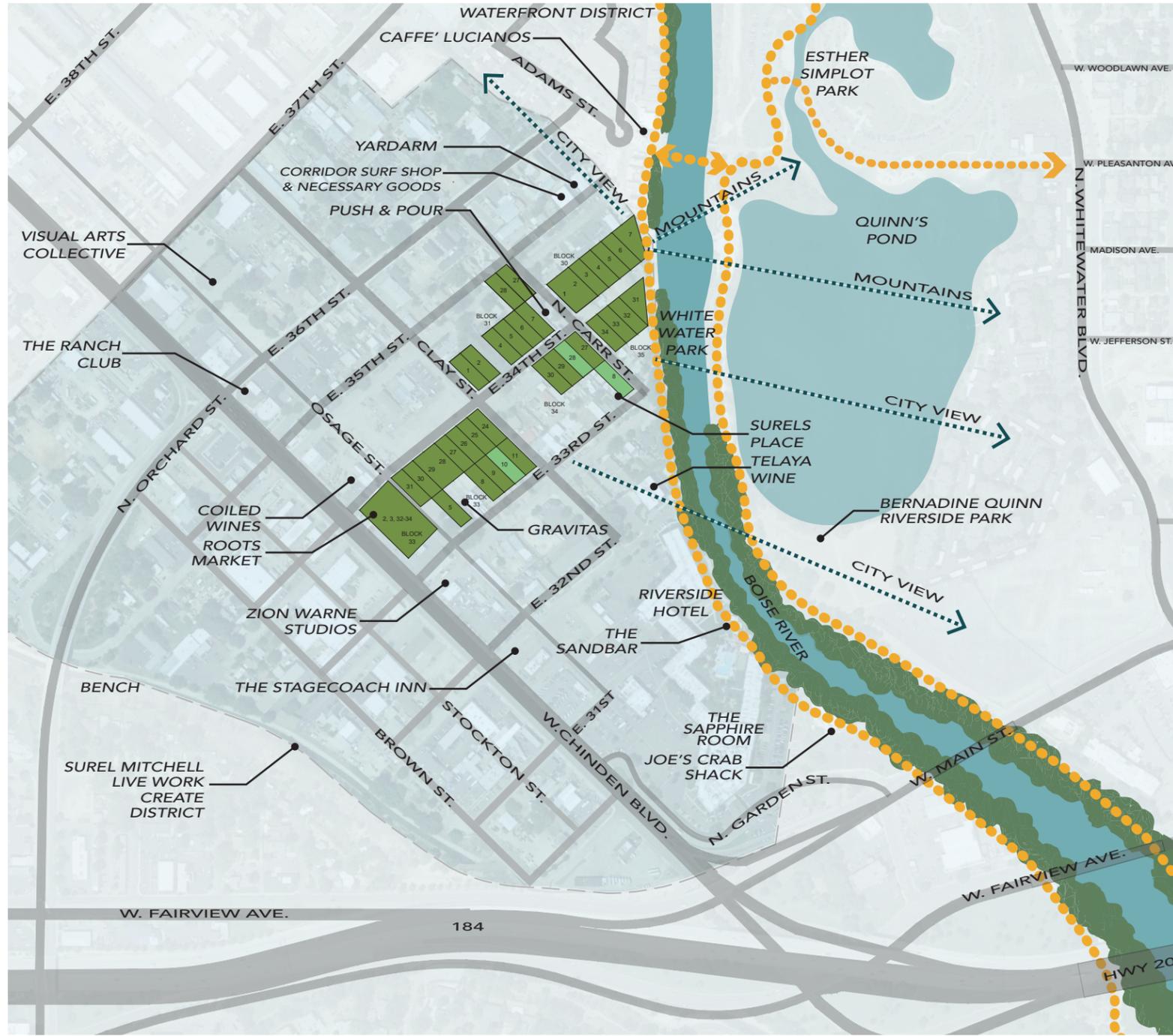


**VICINITY MAP**

This Specific Area Plan is located in the East End of Garden City at the heart of the Surel Mitchell Live Work Create District with 34th Street as the primary corridor connecting Chinden Boulevard to the Boise River Greenbelt. The lots in green are the Owner and Participating Owner properties; we hope adjacent owners will choose to develop similarly.

The map shows the easy connectivity to the I-84 Connector, Highway 20/Chinden Boulevard, Fairview and Main Streets, N. Orchard Street to the Bench, the Greenbelt and Greenbelt bridge across the river, as well as the existing grid of streets within the District.

The 34th Street Specific Area Plan area is close to extraordinary amenities within and near the area – the Boise River, Whitewater Park, Quinn’s Pond and Park, Esther Simplot Park, views to the mountains and city skylines, and the Greenbelt and it’s connections to other river-activated uses. This area has existing businesses that attract Live Work Create activities with artisans, artist studios, events, exhibits, restaurants, drinking establishments, and wineries.



**FIGURE : VICINITY MAP**





## HISTORY, ARTS & CULTURE

The East End of Garden City and the Surel Mitchell Live, Work, Create district provided such a rich history of art, history and Culture. We want to celebrate our history while expanding on the arts and encourage a community of unique identity and a vibrant culture.

Our 34th Street Specific Area Plan will continue to build upon the foundation of current art by providing makers space, and live work units. Each block will offer public art, and so many opportunities for "a call for artists". Even small details of the plan will allow for a wonderful unique art demonstration, bike racks, art fencing, Garden boxes, building lighting. Incorporating the arts into the plan keeps our district unique and offers opportunities to local artists to showcase their work.

History; In this part of Garden City is where activities first started. This was the original town site and is full of historical references. We would like to have future conversations with several local groups to establish a Historical team ( or workshop)that can help select images to reference our history.

In the exhibition house we will also have a museum wall to nightlight the artifacts we have collected.

Our Culture; This is a vibrant community having the ability to have community functions and community events strengthens our culture. Providing spaces where people can gather and have events has been carefully planned and many demographics have been considered.



**PARTICIPATING OWNERSHIP**

It is the vision of a particular one, and the efforts of many, to create this very creative, exciting urban node connecting 3 blocks from the heavily trafficked Chinden Boulevard to the exciting Boise River and greenbelt. The primary property owner has been joined by 'participating' owners who will abide by the plan, and with the hope of future adjacent owners developing in a supportive manner in the future.

**Urban Willow (Ownership) Lots:**

- Fairview Acres Sub. No.5
- Block 30, Lots 1-7
- Block 31, Lots 1,2,4-7,27,28
- Block 33, Lots 2,3,5,8,9,11,24-34
- Block 34, Lots 27, 29, 30
- Block 35, Lots 31-34

**Participating Lots:**

- Fairview Acres Sub. No.5
- Block 33, Lot 10 (Dana Kevin Bowes)
- Block 34, Lots 8, 28 (Ahlberg East LLC)

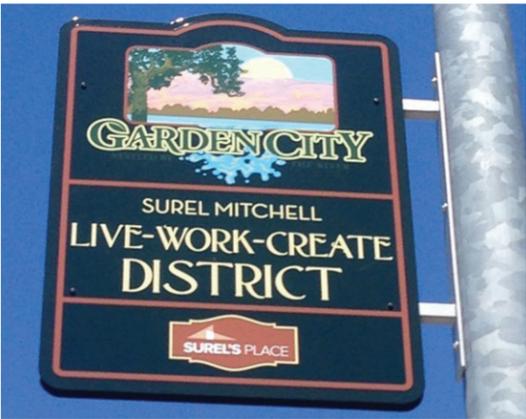
**Legend**

- URBAN WILLOW LOTS
- PARTICIPATING LOTS
- BOISE GREENBELT



**FIGURE : PARTICIPATING OWNERSHIP MAP**





**EXISTING ZONING**

Existing properties are currently zoned one of these categories:

- C1 - Highway Commercial
- C2 - General Commercial
- M - Mixed Use

Most of the adjacent properties in the area are zoned C-1 along Chinden Boulevard or C-2, and are occupied primarily by businesses. In addition to the current zones, the properties are within the Surel Mitchell Live Work Create District Overlay and the recently created Boise River Greenbelt District Overlay. Each of the zones and overlay districts has its own set of requirements for allowed uses, setbacks, building height, etc, making it very difficult to create a cohesive urban neighborhood node in the East End of the City. However, the City's Specific Area Plan Ordinance and Planned Unit Development application process allows for such a creation provided the goals of the City are met.

FIGURE : EXISTING ZONING

**Legend**

- C-1 - HIGHWAY COMMERCIAL
- C-2 - GENERAL COMMERCIAL
- M-U - MIXED USE
- BOISE RIVER GREENBELT ORDINANCE OVERLAY AREA
- SUREL MITCHELL LIVE WORK CREATE DISTRICT AREA



**BASE FLOOD ELEVATION (BFE) MAP**

The 34th Street Specific Area Plan is located in the Boise River flood plain, and is currently located in Zone X which has 0.2% annual chance of flood hazard. The river channel has undergone changes and a new study is proposing that this site be changed to a Zone AE designation with a 1% annual chance of flooding at a determined Base Flood Elevation (BFE).

The proposed BFE is three to four feet above the existing road centerline elevations on the site. For insurance purposes we are proposing raising most of the finish floor elevations of the living and commercial structures one foot above the proposed BFE thereby mitigating the potential for loss due to flooding (Garage floors may be placed below the BFE).

At the plaza at the Greenbelt, the area will be raised above the BFE, with the parking garages below the first floor and below the BFE, which is allowable.

The Ultimate Parklet is a unique location that will serve people on the street and needs to be at street level. The businesses that will occupy the parklet will need to accept the insurance costs and flood risk to gain the access to the street level.

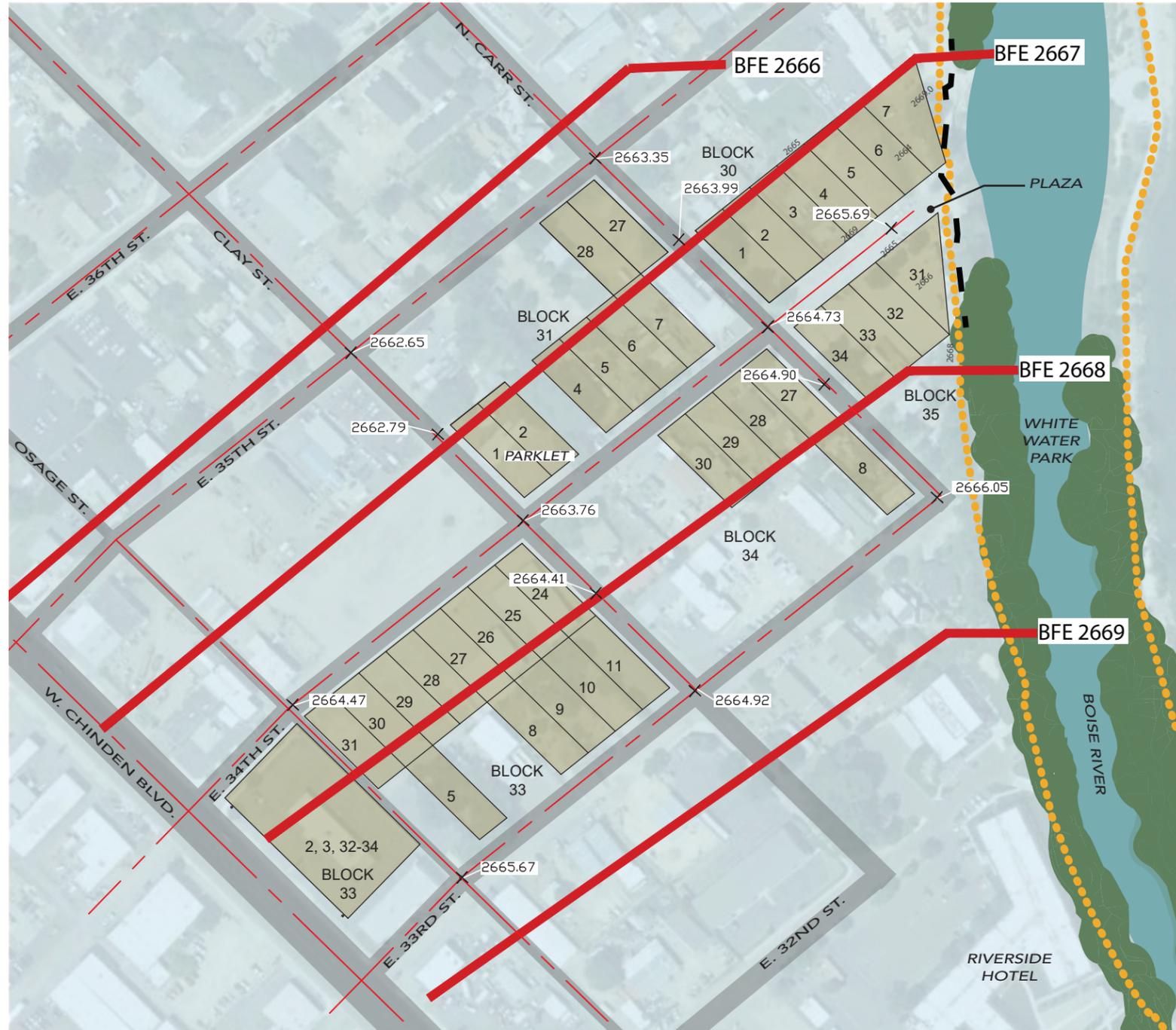


FIGURE : BFE MAP





EXISTING TRANSIT & BIKE HUB MAP

### EXISTING TRANSIT MAP

Valley Regional Transit operates bus transit in the neighborhood with Route 11 stops on Chinden Boulevard. Garden City Codes allow and incentivize development within 1/2 mile radius. Here we have shown easy walkable 1/4 mile radii from the stops.

#### Legend

-  ROUTE 11 BUS STOPS
-  ROUTE 11
-  BOISE GREENBELT
-  EXISTING BOISE GREEN BIKE HUB



## EXISTING ON-STREET & OFF STREET PARKING

The map and chart below indicates:

- Existing on-street parking in vicinity of the plan
- Existing on-street parking adjacent to Participating lots
- Existing off-street parking on Participating lots
- Existing off-street parking on City Lot at 36th Street

## EXISTING VICINITY PARKING

ON-STREET PARKING		
STREET	# ON-STREET PARKING SPACES ADJACENT TO NON-PARTICIPATING LOTS	# ON-STREET PARKING SPACES ADJACENT TO PARTICIPATING LOTS
32nd	33	0
33rd	24	0
34th	17	60
35th	65	2
36th	20	0
CLAY	46	17
CARR	27	27
TOTAL	232	106
TOTAL ON- STREET PARKING SPACES		338
OFF-STREET PARKING		
CITY LOT OFF-STREET PARKING		24
TOTAL ON-STREET & OFF-STREET PARKING		362

NOTE:

- STREET PARKING SPACES ARE MEASURED AT 20' X 10'
- NO PARKING WITHIN 15' OF A FIRE HYDRANT
- NO PARKING WITH 20' OF A CROSSWALK
- NO PARKING IN FRONT OF PUBLIC OR PRIVATE DRIVEWAYS
- NO PARKING WHERE INDICATED BY SIGNAGE

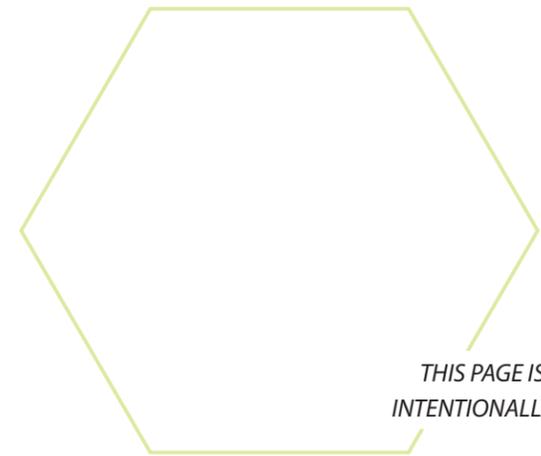
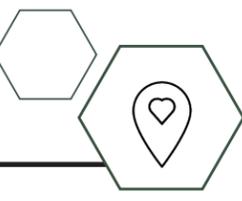
## Legend

●●●● BIKE & PEDESTRIAN GREENBELT



FIGURE : EXISTING ON-STREET & OFF-STREET PARKING





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### LAND USE PLAN

To create a rich and vibrant urban node, a mix of uses, activities, building types, and open space is desired. Most of the blocks within this 34th Street Specific Area Plan are Mixed-Use buildings with commercial on the first floor and residential above. For diversity and affordability of housing types, there are also several blocks of Medium Density Residential buildings with accessory other uses. Open Space is focused on the river front blocks, with additional smaller spaces nestled into other blocks. Each of the Open Space areas has an ability to engage both residents and visitors in creative activities, exhibits, performances and celebrations as befits the Surel Mitchell Live Work Create District.

The Development Chart on the next page indicates the general uses and areas in a condensed, straight forward table. Further in this document, the Detailed Block Plans, Landscape Plans and Design Guidelines will bring life to the blocks while describing in detail setbacks, heights, parking and amenities.



FIGURE : LAND USE PLAN

#### Legend

- MIXED-USE
- MEDIUM DENSITY RESIDENTIAL & LIVE WORK CREATE
- OPEN SPACE
- OPEN SPACE
- FLEX OPEN SPACE & ROOF GARDENS
- BOISE GREENBELT





USE	BLK	Acres	Dwelling Units DU	Dwelling Unit Density	Commercial Sft
Mixed Use	30, Lots 1-7	1.26	53	42.0du/ac	22,000
Med Density Multifamily	31, Lots 4-7, 27, 28	1.04	19	18.3du/ac	0
Parklet and Parking	31, Lots 1,2	0.34	0		200
Commercial	33, Lots 2,3,32-34	0.86	0		30,000
Med Density Multifamily	33, Lots 5, 8-11	0.87	19	21.8du/ac	1,000
Mixed Use	33, Lots 25-31	1.22	57	46.7du/ac	12,000
Commercial	33, Lot 24	0.17	0		8,000
Commercial	34, Lot 8	0.17	0		10,000
Commercial	34, Lot 27	0.17	0		8,000
Commercial	34, Lot 28	0.17	0		9,000
Med Density Multifamily	34, Lots 29, 30	0.34	6	17.6du/ac	0
Mixed Use	35, Lots 31-34	0.72	48	66.7du/ac	16,000
		<b>7.33</b>	<b>202</b>	<b>27.5du/ac</b>	<b>116,200</b>

FIGURE : DEVELOPMENT CHART

Allowed Uses	
Accessory use*	P
Artist studio*	P
Bed and breakfast*	P
Bicycle Sales and Service	P
Daycare, center*	P
Daycare, neighborhood*	P
Daycare, personal*	P
Drinking establishment, full service*	P
Drinking establishment, limited service*	P
Dwelling unit, accessory*	P
Dwelling unit, multiple-family*	P
Dwelling unit, single-family attached	P
Dwelling unit, single-family detached	P
Dwelling unit, two-family	P
Eating establishment, full service	P
Eating establishment, limited service	P
Financial institution*	P
Food products, small scale processing*	P
Food store*	P
Health club*	P
Home occupation*	P
Parking facility	P
Personal service*	P
Professional service*	P
Research and development	P
Retail production*	P
Retail store	P
Temporary use*	P

P = Permitted Use. All other uses are not allowed.

\*Indicates uses that are subject to specific land use provisions as set forth in 8-2C of GC Code.

FIGURE : ALLOWED USES CHART

## PEDESTRIAN, BIKE & VEHICLES MAP

The gem of the pedestrian and bike system is the Boise River Greenbelt, connecting cities up and down the valley. Here it is a wide paved path along the river's edge with indigenous trees in some places, plus open vistas of the Whitewater Park, Quinn's Pond, and mountains in the distance. This 34th Street Specific Area Plan is designed to be pedestrian and cycle centric. From the Greenbelt path, people will be invited in to linger or engage in plaza activities, so pedestrian access is prioritized, with bicycle traffic as 'dismount only' through the plaza. If wanting to move quickly into the neighborhood bypassing the plaza, there is a bike pathway at the southern perimeter. Once at N Carr St or 34th Street, they enter a multimodal grid system.

The East End of Garden City is blessed with an historic street system – a roughly rectangular grid dating from over a century ago. The grid enables many options for navigating the neighborhood. While 5-lane Chinden Boulevard is dense with heavy traffic, the neighborhood streets in this area are 'local' streets serving local residences and businesses. For much of this Plan we propose to use the City standard street section with detached sidewalks, with the notable exception of 34th Street and Carr Streets, as they will have the most intense mixed use development. For those streets we propose a more urban, multimodal shared vehicle and bike street (sharrow) with wider attached sidewalks adjacent. All of the intersections will feature 'bulbed-out' sidewalks at corners to make pedestrian crossing shorter and safer. Along the sidewalks will be benches, public art, planters, light poles and other elements for pedestrian comfort and engagement. Parallel vehicle parking is on both sides of the street with trees planted in bulb-outs periodically along the streets between parking spaces. E. 34th and N. Carr Streets also act as emergency aerial apparatus access to the taller buildings.

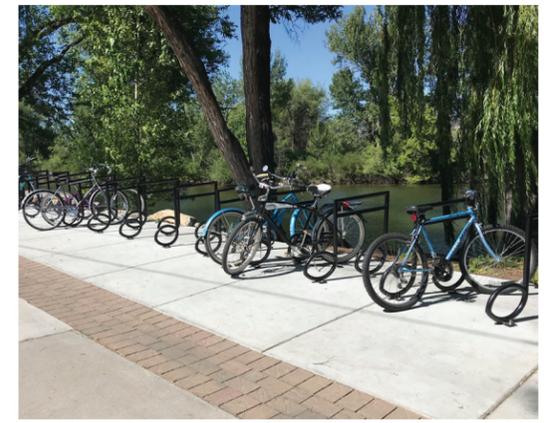
The Internal Circulation routes serve the individual block inhabitants with parking access, solid waste removal, and as a means to circulate to Clay and Carr Streets when farmers markets and other events occasionally close 34th Street.

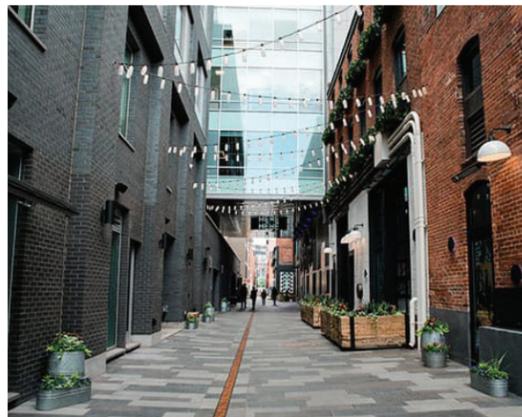


**FIGURE : PEDESTRIAN, BIKE & VEHICLES**

**Legend**

- SHARED BIKE & VEHICLE STREETS WITH ON-STREET PARKING
- MULTI-LANE VEHICLE 'GREEN BOULEVARD' STREET WITH WIDE SIDEWALKS
- BIKE & PEDESTRIAN ONLY
- PEDESTRIAN ONLY/BIKE DISMOUNT AREA
- BIKE & PEDESTRIAN GREENBELT
- INTERNAL CIRCULATION
- EMERGENCY VEHICLE ACCESS
- SERVICE VEHICLE ACCESS
- SERVICE VEHICLE & BICYCLE ACCESS





## SERVICE & LOADING

Service and loading for buildings is delivered through 20' wide drive aisles located on the rear and sides of blocks as indicated. These 'internal circulation routes' serve the individual block inhabitants with parking access, solid waste removal, and as a means to circulate to Clay and Carr Streets when events occasionally close 34th Street.

Emergency Service drives are indicated at the river-activated blocks along the east and west sides; the west drive continues through the plaza and down pedestrian-only corridor at 34th Street. Access at 34th and Carr Streets to the pedestrian-only corridor will be controlled with bollards.

Osage Street is essentially an alley service drive and provides access to parking at Block 33 as well as solid waste removal.

### Legend

- SERVICE & LOADING ROUTES AT DRIVE AISLES & INTERNAL STREETS
- EMERGENCY ACCESS
- BOISE GREENBELT

FIGURE : SERVICE & LOADING





### VEHICLE & BIKE PARKING MAP

The 34th Street Specific Area Plan's strategy is to support the Comprehensive Plan and promote walking, biking, and transit in this well connected area, while providing adequate vehicle parking and a surplus of bicycle parking.

Most visitors will come to the area via the greenbelt and local residents will be walking and biking locally to the uses we will be providing. All paking is in compliance with Live Work Create Overlay requirements.

In addition, Garden City Code allows no off-street parking required for working spaces less than 500 square feet of interior floor area, and suggests reduced parking for the following:

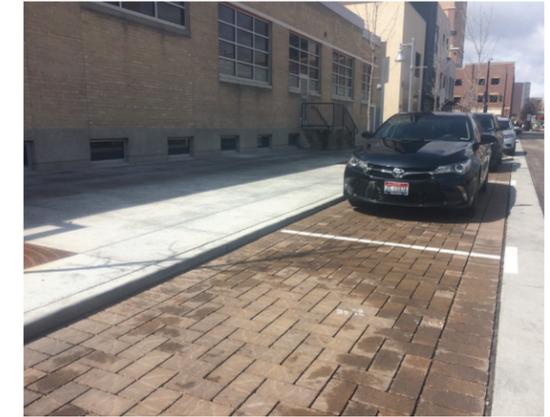
- Shared parking with convenient pedestrian connections between separate properties.
- The properties and/or uses are within one-fourth (1/4) mile of each other.
- Directional signs to provide notice of the availability of parking.
- There is a transit stop within one-fourth (1/4) mile of all uses.
- There are shower facilities and lockers to support and encourage the use of non-motorized transportation by employees- We will provide these at the large river buildings, as well as showers and changing rooms at the Ultimate Parklet.
- There is availability of on-street parking, shared, and/or public parking within the vicinity of the use - We have increased the amount of on-street parking, shared, and/or
- Public parking within the vicinity of the public uses will be provided on street and at the Ultimate Parklet public parking lot. This lot may transform to a parking structure in the future if more public parking is necessary, and transform again to other uses when not needed in the future.
- The Chinden Boulevard MarketPlace vehicle parking lot will be made available for event parking.

#### Legend

- PRIVATE PARKING
- PUBLIC ON-STREET PARKING
- PUBLIC BIKE PARKING AT PARTICIPATING LOTS
- PROVIDED PUBLIC OFF-STREET PARKING
- STREET LIGHTS
- BIKE & PEDESTRIAN GREENBELT



FIGURE : PROPOSED VEHICLE & BIKE PARKING





USE	BLK	Lots	D.U.	On-Site Spaces for D.U.	Commercial s.ft	On-Site Spaces for COM	On-Street Spaces	Private Bike Parking Inside	Public Bike Parking Outside
Mixed Use	30	1 -5	52	52	20,000	26	1	80	24
Mixed Use	30	6 -7	1	2	2,000	0	0	2	0
Med Density Multifamily/Live/Work	31	4 -7	12	20	0	0	8	8	16
Med Density Multifamily/Live/Work	31	27	3	3	0	0	4	3	0
Med Density Multifamily/Live/Work	31	28	4	4	0	0	2	0	0
Parklet and Parking	31	1 -2	0	0	200	24	6	0	17
Mixed Use	33	2,3,32-34	0	0	30,000	42	3	0	43
Med Density Multifamily/Live/Work	33	5	3	3	1,000	1	2	0	5
Med Density Multifamily/Live/Work	33	8 -11	16	16	0	4	14	0	30
Mixed Use	33	24	0	0	8,000	4	4	2	18
Mixed Use	33	25 - 31	57	57	12,000	0	14	10	37
Commercial	34	8	0	0	10,000	4	6	3	16
Commercial	34	27	0	0	8,000	4	5	4	13
Commercial	34	28	0	0	9,000	5	2.5	4	0
Med Density Multifamily/Live/Work	34	29- 30	6	6	0	0	2.5	6	18
Mixed Use	35	31 - 34	48	48	16,000	12	1	72	36
			202	<b>211</b>	116,200	<b>126</b>	<b>75</b>	<b>194</b>	<b>273</b>
<b>TOTAL PARKING</b>					<b>412 Vehicle Spaces</b>			<b>467 Bicycle</b>	

110 required bicycle spaces per code

FIGURE : PARKING CHART



**STREET SECTIONS**

The 34th Street Specific Area Plan will have two different street section upgrades not only to add curb, gutter and sidewalks, but to provide an exceptional transportation experience that promotes human interaction and unifies the street beyond just a thoroughfare into a type of 'outdoor room'. The Attached Sidewalk is in the more active mixed-use urban environment and contains pedestrian and cyclist comfort amenities such as trees, benches, garden boxes, public art, and bike racks. The Detached Sidewalk Street Section fronts more residential and Live/Work uses with a planting strip to beautify curb area for pedestrians.

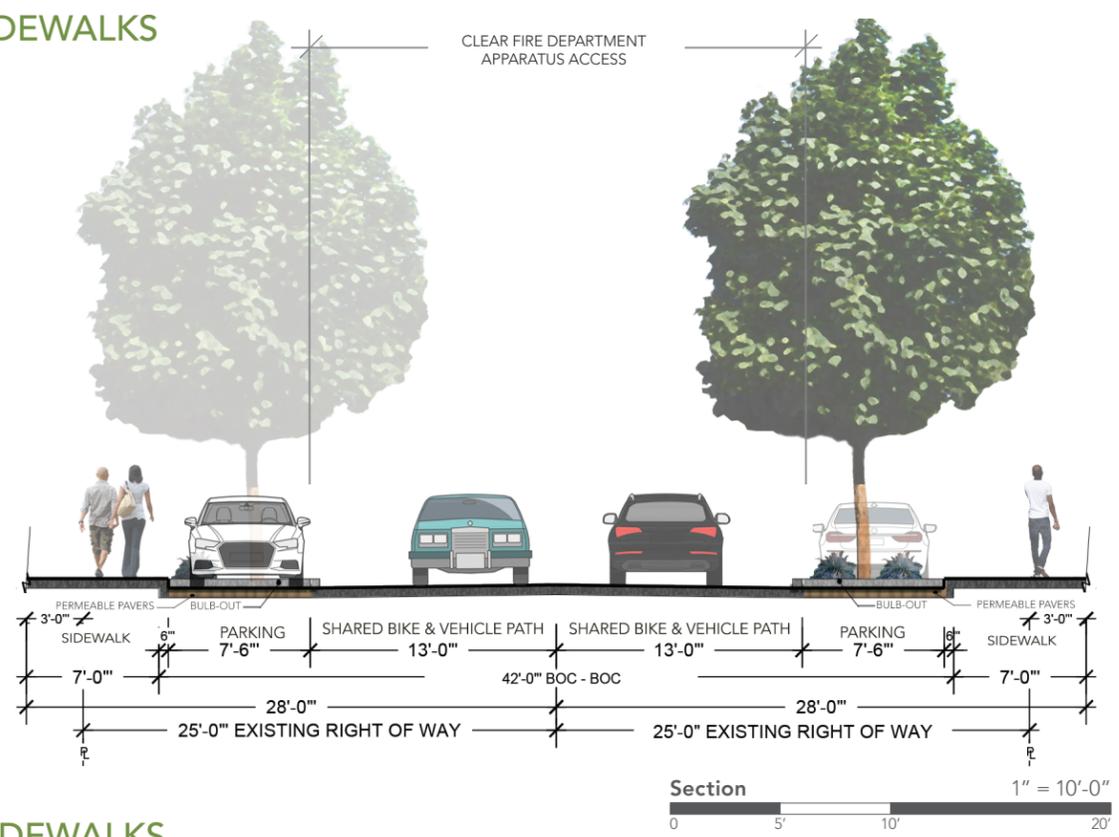
The Attached Sidewalk Street Section has a 42' wide street from Back of Curb (BOC) with two shared 13' wide shared vehicle and bicycle lanes (sharrow) and parallel parking on both sides. The drive lanes are paved in asphalt with the parking aisles paved in permeable pavers for stormwater drainage. The drive lanes accommodate the Fire Department's aerial apparatus when used for emergency access to buildings over 30' tall. A 7' wide attached sidewalk will accommodate street furnishings, public art and custom bikes racks to enrichen the pedestrian/cyclist experience. 'Bulb-outs' with trees will occur periodically down the streets, as well as street intersections. These bulb-outs will be spaced at 22' from the road centerline. This section will be used at E. 34th Street and N. Carr Street.

The Detached Sidewalk Street Section is Garden City's Standard Section has a 36' wide street from Back of Curb (BOC) with two vehicle and bicycle lanes and parallel parking on both sides. The drive lanes are paved in asphalt with the parking aisles paved in permeable pavers for stormwater drainage. A 5' wide concrete sidewalk sits behind a 6' planting strip next to the vertical curbs. The planting strip here will not be planted with lawn but with trees and drought tolerant low shrubs. The trees will be within a root barrier. This section will be used at 33rd Street, 35th Street, and Clay Street.

See Landscape Detailed Street Sections for more information on streetscape furnishings, public art and plantings.

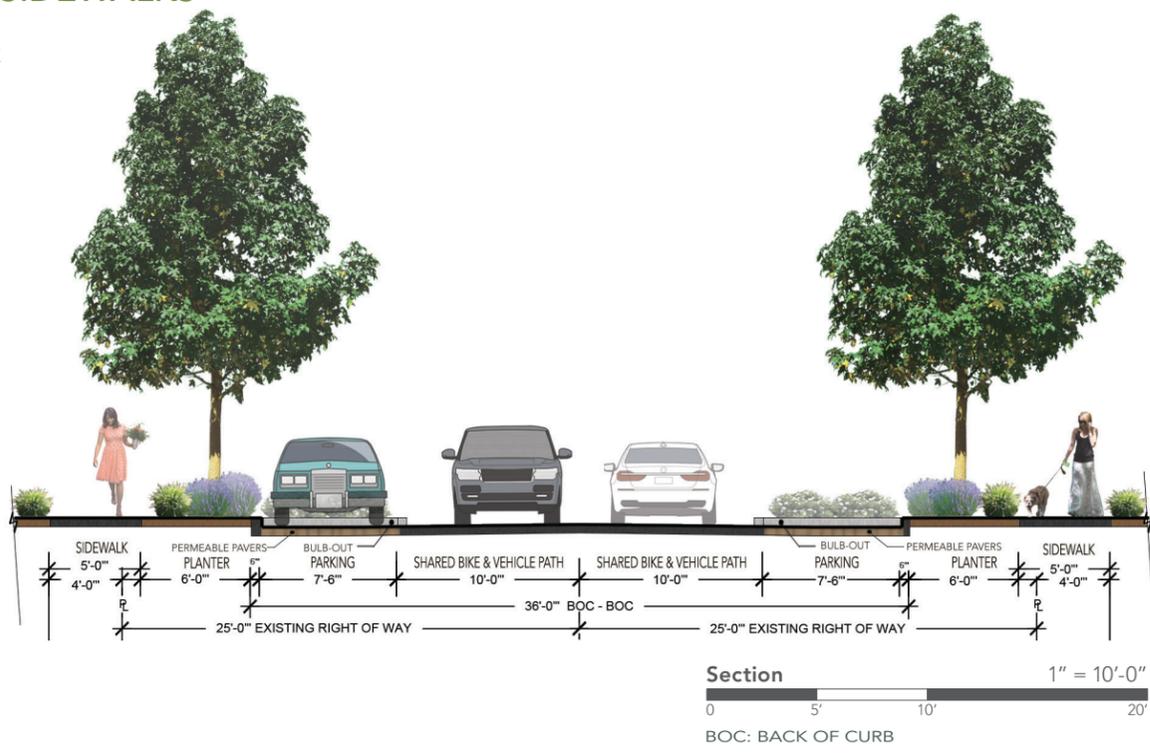
**ATTACHED SIDEWALKS**

**34th Street & Carr Street**



**DETACHED SIDEWALKS**

**33rd, 35th & Clay Street**





EXISTING TRANSIT & BIKE HUB MAP



## TRANSIT & BIKE HUB MAP

Valley Regional Transit bus transit Route 11 currently stops on Chinden Boulevard. We propose to add a future loop on E. 34th Street to N. Carr St to E. 36th Street to bring better access to the neighborhood.

Three bike hubs provided by Boise Green Bike Hubs are shown on 34th Street including one at the river plaza.

### Legend

-  PROPOSED ROUTE BUS STOPS
-  EXISTING ROUTE 11 BUS STOPS
-  PROPOSED ROUTE
-  EXISTING ROUTE 11
-  BOISE GREEN BIKE HUB
-  BOISE GREENBELT

## STREET LIGHTING

Street lights are an integral part of a streetscape and within a community. Their function and purpose promotes safety in various forms, aesthetic opportunities, and provide a unified character that reinforces the theme of a place. Below is an overview of the lighting requirements for commercial and residential developments, and what is proposed for the 34th Street Specific Area Plan.

Per Garden City's Street Lighting Operations & Maintenance Manual, the following information is provided:

### PURPOSE:

The purpose of this document is to promote uniformity within the City's street lighting system.

Properly designed street lighting systems will:

- increase visibility and safety in locations where vehicular traffic and pedestrian traffic share the right-of-way,
- increase visibility and safety for vehicular traffic at intersections and other areas of potential hazard,
- act as a deterrent for certain types of crime, thereby increasing general public safety, and
- Provide for aesthetics in land use designations such as the Live Work Create

### REQUIREMENTS

Residential Local & Collector Streets:

Fixtures: 100 watt High Pressure Sodium

Height: 20' Local Streets, 25' Collector Streets

Locations where street lighting will improve public safety

Pedestrian crossings, steep embankments, sharp curves, public bike paths, dead-end streets, cul-de-sacs, bridges, steep inclines, intersections

And, other locations that may affect public safety and/or security

After the above criteria is met, lighting with 400' minimum spacing

Lower lighting levels allowed for residential uses to minimize negative implications

The 34th Street Specific Area Plan will provide the minimum street lighting standards as well as design the lighting standard to reduce light pollution. At the time of construction drawings, accurate placement of street lights and building lights will be established. Proposed pedestrian lights are provided at all pedestrian crossings and other required locations specified, at a minimum spacing of 100' and beyond the minimum requirement of 400' minimum spacing. These lights are to be the minimum 25' tall height located along the sidewalks and in planters, and offer an aesthetic, that help unify the character of the overall development that will promote Garden City's liveability and vibrancy.

See conceptual images to the right of the proposed street light types.

### Legend

-  PEDESTRIAN STREET LIGHTS
-  INTERSECTION STREET LIGHTS
-  BIKE & PEDESTRIAN GREENBELT

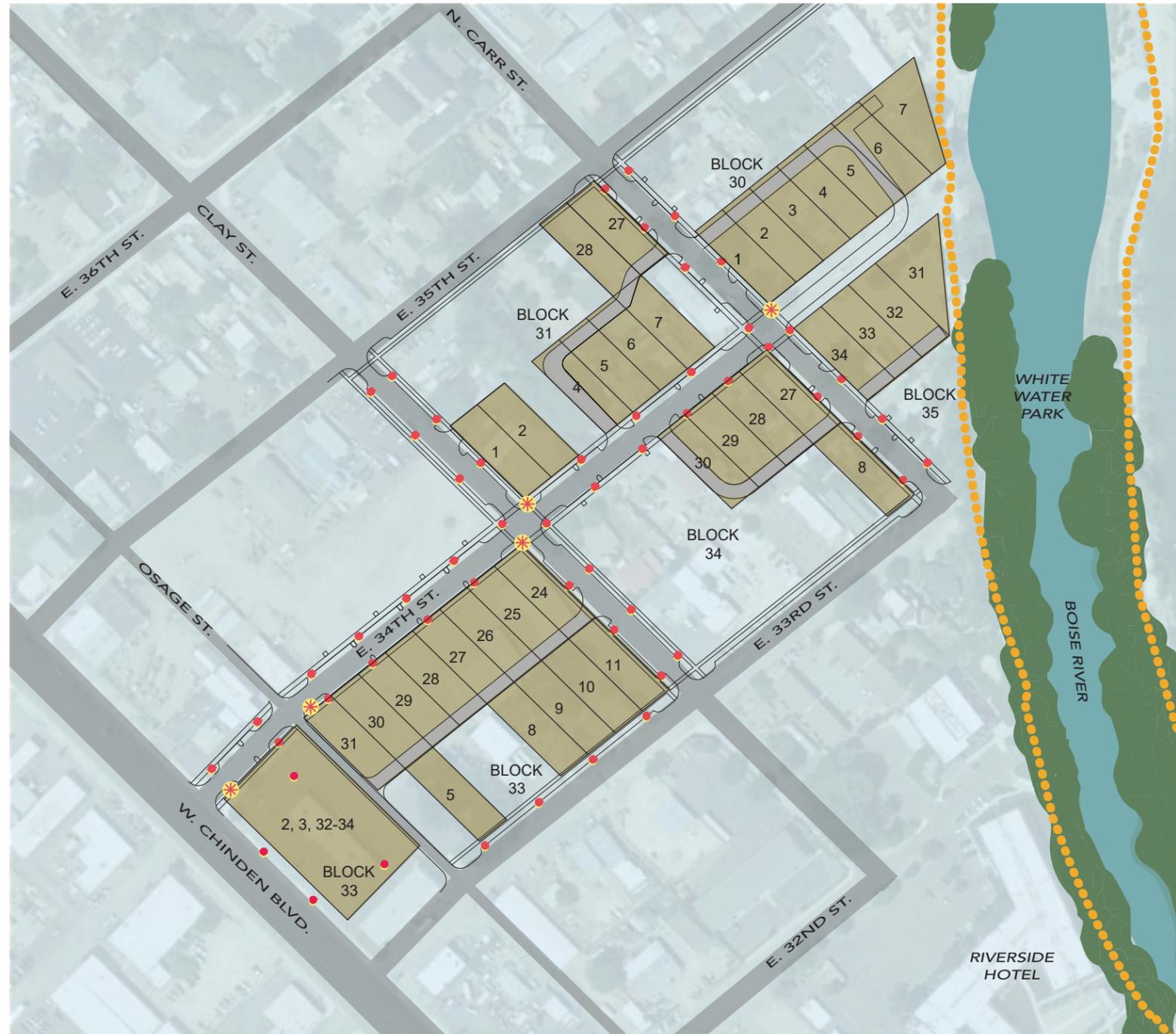


FIGURE : STREET LIGHTING





FORMS & SURFACES COLUMN LIGHT



FORMS & SURFACES COLUMN LIGHT



FORMS & SURFACES TRIO LIGHT



FORMS & SURFACES TRIO LIGHT



LANDSCAPE FORMS ALCOTT 5 AREA LIGHT



LANDSCAPE FORMS ALCOTT 5 AREA LIGHT



FORMS & SURFACES CORDIA LIGHT



FORMS & SURFACES CORDIA LIGHT



LANDSCAPE FORMS LEO 28 LIGHT



### POWER, COMMUNICATIONS, GAS

Power services are located along the rear lot lines of Fairview Acres No. 5. A joint trench for multiple utilities including natural gas and cable is anticipated along the rear lot lines.

Idaho Power Company currently serves the area with overhead power. With major new construction, power is required to be underground. We will be installing underground power in the existing easements to our projects. We are open to discussions about moving all power services underground with the cooperation of the existing neighbors and possible participation of Idaho Power Company or other parties.



#### Legend

- JOINT TRENCH
- BIKE & PEDESTRIAN GREENBELT

FIGURE: POWER, COMMUNICATION, GAS





### SEWER & WATER

Currently there is adequate water available from the existing 12 inch waterline in N. Carr Street. The 12 inch water line will be extended as the development progresses (See phasing Plan).

Currently an 8 inch sewer line exists in 34th Street and flows to Osage. The sewer line at Osage is a 10 inch line and flows northwesterly under Osage to 36th Street where it increases to 12 inch.

We will be working with Garden City Public Works as properties are determined to analyze sewer requirements and sewer capacity.

#### Legend

- EXISTING WATER LINE
- EXISTING SANITARY SEWER LINE
- EXISTING BOISE CITY SANITARY SEWER INTERCEPTOR
- BIKE & PEDESTRIAN GREENBELT

FIGURE : SEWER & WATER





### STORMWATER & DRAINAGE

All site storm water will be retained and infiltrated on the site. This site has high seasonal ground water which makes the usual subsurface methods of storm water retention and disposal environmentally unacceptable. Storm water will be retained and infiltrated at the surface to provide maximum treatment prior to mixing with ground water. Retaining and infiltrating at the surface is the most environmentally friendly method of storm water disposal available for this site. Other benefits of the new system are that the reconstruction of the streets and storm water systems will remove storm water from the current system which flows to the Boise River with little treatment. The proposed permeable pavers add interest and beauty to the site.

Storm water is treated as two entities: Storm water that falls on privately owned lots is the responsibility of the lot owner and will be retained on the lot. The water will be directed to the parking areas in the rear of the buildings where the storm water will be infiltrated with a permeable paver system. Storm water that falls within the right-of-way is considered public water and is the responsibility of the ACHD. Public storm water will be retained in the right-of-way and will be infiltrated with permeable pavers in the parking strips along the sides of 34th St.

#### Legend

-  PROPOSED DRAINAGE DIRECTION
-  PROPOSED PRIVATE DRAINAGE WITH PERMEABLE SYSTEMS
-  PROPOSED ACHD DRAINAGE WITH PERMEABLE PAVERS OR RAINSTORE3 UNDERGROUND WATER STORAGE SYSTEM
-  BIKE & PEDESTRIAN GREENBELT



FIGURE : STORMWATER & DRAINAGE





### EXISTING IRRIGATION

Existing irrigation is provided through water users associations exists though open ditches or pipes as shown. Irrigation water is available to all the lots. The ditches have been unused and filled in in many places. The ditches could be reopened if someone wanted water. The point in installing pipe where the ditches are filled is to regain the access to the water.

#### Legend

- EXISTING GRAVITY IRRIGATION PIPE
- EXISTING OPEN IRRIGATION DITCH
- EXISTING FILLED & UNUSED IRRIGATION DITCH



FIGURE : EXISTING IRRIGATION



### PROPOSED IRRIGATION

All of the existing open, used, unused, and filled ditches on owned lots are converted to irrigation pipe with exception of the existing piped ditch at the south of Block 33, Lot 5 is converted to an open ditch. Where irrigation ditch users water is not available, city water will be used.

**Thurman Mill Ditch Proposal**  
The Thurman Mill Ditch diversion is currently an open ditch that takes its water from the Boise River just above the Whitewater Park wave structure. The open ditch is approximately ten feet deep and thirty feet wide. The control gates are unprotected. The Boise River Greenbelt Bridge crosses the ditch between the diversion structure (3) and the control gates (1). We believe the ditch to be an unattractive nuisance and a hazard. With the increased usage of the area we feel the ditch should be made safer and beautified. We are proposing to cover the ditch from the bridge to the control gates and add a trash rack to protect the entrance. Access under the bridge from the westerly side would be removed. Access to the control gates would be protected with an "Art Barrier" that would allow access for maintenance but protect area patrons. The area would be landscaped and allow for access and use. The control house (2), control gates and box (1), diversion structure and trash racks (3), and the Greenbelt Bridge will remain unchanged. We are currently working with Thurman Mill Ditch to meet their needs with the proposed changes.

#### Legend

-  PROPOSED OPEN IRRIGATION DITCH
-  PROPOSED IRRIGATION PIPE LOCATION
-  PROPOSED THURMAN MILL IRRIGATION PIPE
-  EXISTING IRRIGATION CONTROL HOUSE LOCATION



FIGURE : PROPOSED IRRIGATION

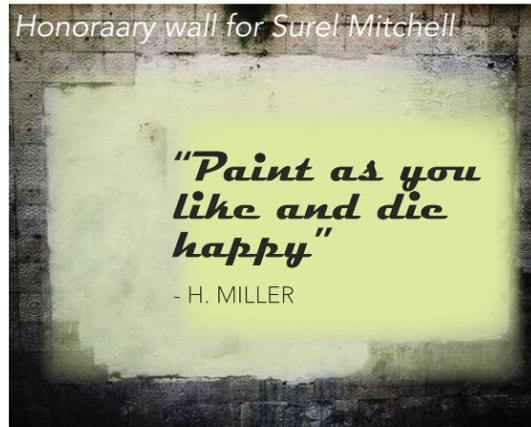


FIGURE : PUBLIC ART, CULTURE & HISTORY MAP



## PUBLIC ART, CULTURE & HISTORY

The 34th Street Specific Area Plan will celebrate the Public Art, Culture, and History of Garden City through art and interpretive displays located throughout the area as indicated in the map. Two monumental creations will signify the gateway entries important to this District – one at Chinden Boulevard and again at the plaza entrance at Carr Street.

To facilitate creations that honor the past and embody the current culture and artistic oeuvre or collection of art in Garden City, a 'call for artists' will be established with opportunities to collaborate with the Garden City Arts Commission and Surel Mitchell to ensure a desired selection outcome and call for artists process.

### Legend

-  ENTRY FEATURE LOCATIONS
-  FUTURE LOCATION FOR PUBLIC ART, CULTURAL AND/OR HISTORICAL INTERPRETIVE FEATURES
-  OPEN SPACE
-  FLEX OPEN SPACE & ROOF GARDENS
-  BIKE & PEDESTRIAN GREENBELT



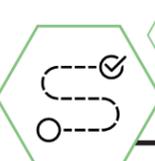
- COLIED WINES
- RAISED TERRACE AT BUILDING FRONTAGE
- COMMUNITY GARDENS
- PERMEABLE PAVERS
- PLAZA
- GATEWAY ENTRY FEATURE
- PARKING & PERMEABLE PAVERS
- MARKET PLACE



### SPECIFIC AREA PLAN/MASTER PLAN

The 34th Street Specific Area Plan brings to life the vision at a high level view. It shows the relationship between the streets, buildings, open spaces, and immediate amenities such as the greenbelt and river/whitewater park, providing a visual of how the development will be tied together. One can identify that an experience rather than a passage is provided on the streetscapes such as 34th Street from the Marketplace, along the Mixed-Use, Live/Work Townhomes, all the way to the Eddy Plaza where the greenbelt and river/whitewater park are celebrated. The experience is promoted through the various materials from permeable pavers, sculptures and other artworks, trees and planters, building materials, lighting. Ease of access and multi-modal circulation promotes users to comfortable drive, bike, and most importantly walk from one end to the other and to take advantage of the many gathering spaces that promote the public/private uses that are so important in the community. Most importantly, the 34th Street Specific Area Plan shows the area is designed and planned for people, and the experience at a human-scale. See Detailed Block Plans.





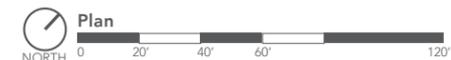
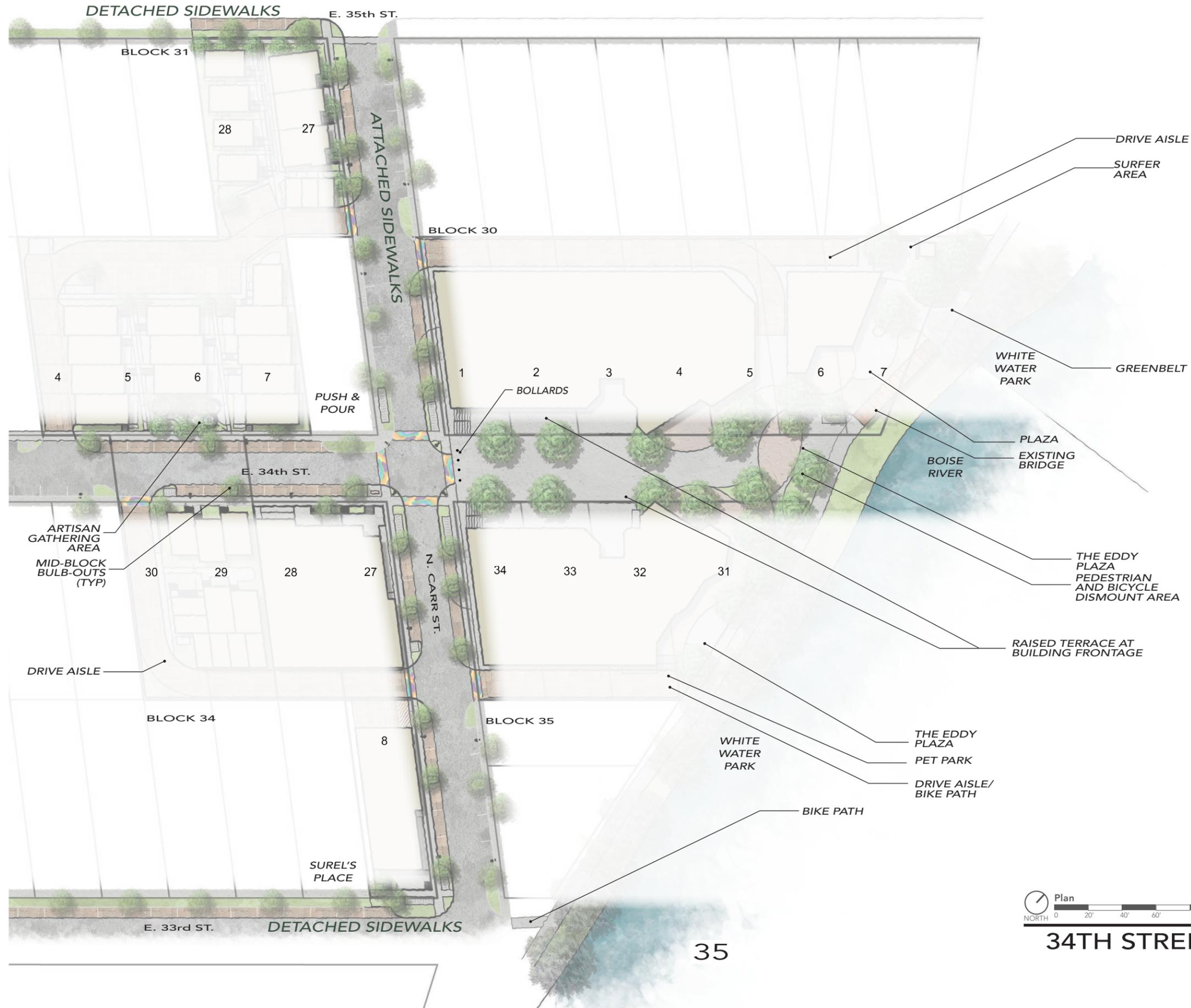
# STREETSCAPE MASTER PLAN

The streetscape theme is tied together and promoted through various materials and elements. Continuity is achieved through repetition of the materials which include permeable paving that provides aesthetic and site drainage functions. Further, benches and bike racks provide use and aesthetic for users passing through or passing time at the various locations within the 34th Street Specific Area Plan limits. The streetscapes also offer various trees that promote a feeling of safety from the street for pedestrians, and a comfort of shelter with the overhead plane, while softening the many built features. Planters and community gardens provide a diverse mix of plantings that soften the hardscapes and add a great deal of visual interest. See Detailed Block Plans.

## Legend

-  PERMEABLE PAVERS
-  BENCHES
-  BIKE RACKS
-  SHADE TREES
-  PLANTER
-  PEDESTRIAN LIGHTING





34TH STREET SPECIFIC AREA PLAN

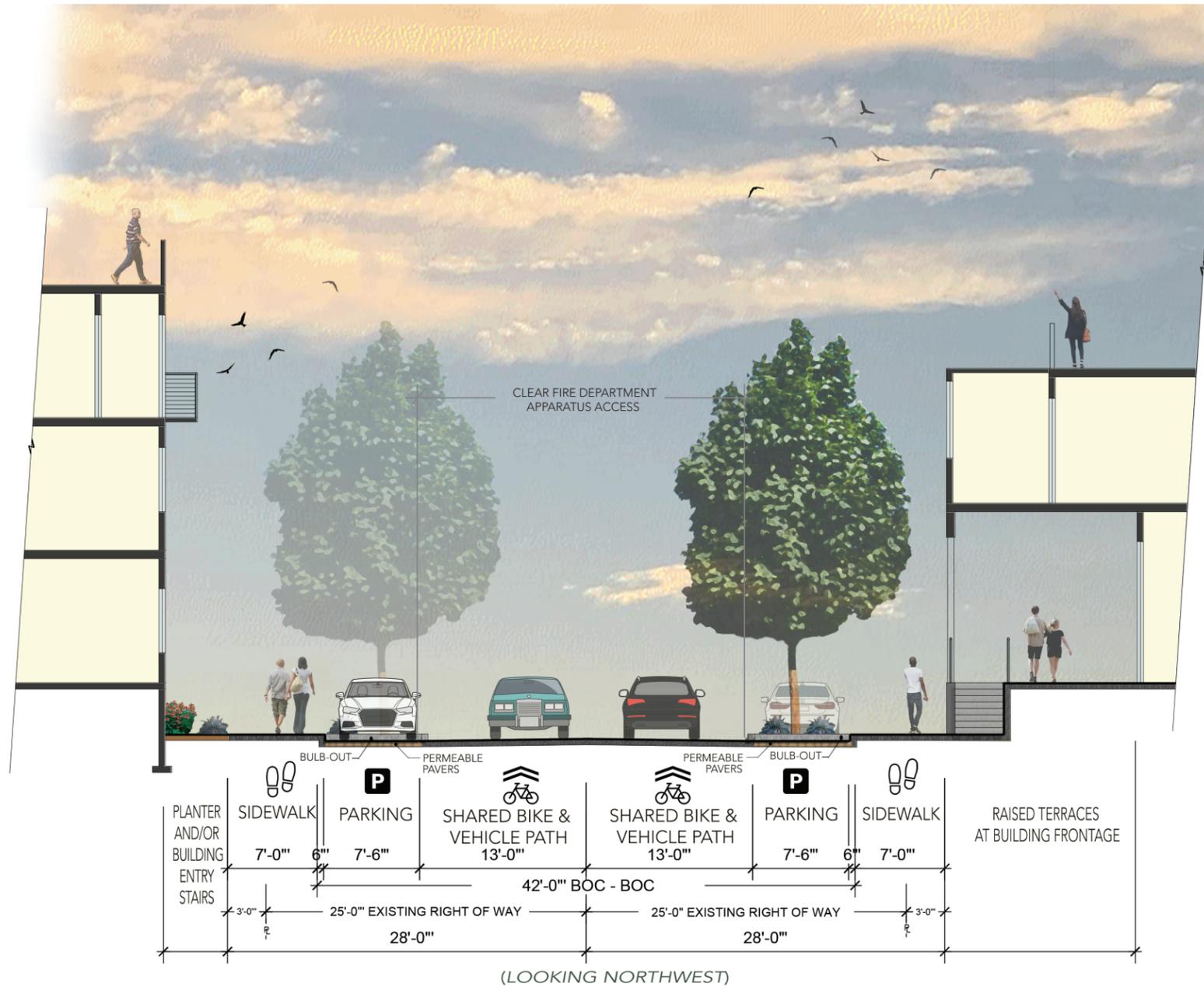
# DETAILED STREET SECTIONS

## ATTACHED SIDEWALK(34th & CARR)

There are two different street section profiles, Attached sidewalks and Detached sidewalks.

Attached sidewalks are proposed along 34th Street and Carr Street. The attached sidewalk promotes a streetscape feel that encourages human interaction with the building frontages which consist of a front zero lot line setback to back of sidewalk. Pedestrians will enjoy the comfort and aesthetic of the amply wide 7' sidewalks lined with Class II street trees at 50' intervals in addition to furnishings on the street side. Cars parked parallel to the sidewalk also add a level of security and safety from the moving cars on the street. Meanwhile raised planters, art nodes, and abundant artistic bicycle parking add interest along the elevated building frontages provided on the other side. Ramps and stairs are provided along the sidewalk enticing passers by to get up to the elevated building frontage level three to four feet above the sidewalk elevation, out of the BFE (Base Flood Elevation). There are various spaces designated for people to gather and relax while enjoying their prospect over the street. Users walking along the sidewalk or gathering up on the frontage level will have the convenience of trash receptacles located along the sidewalks and other strategically located for cleanliness, and benches provided for a spot to relax or wait for a friend or loved one. The raised planters and ground level planters provide optimal space for functional and aesthetic plantings to soften and add to appealing sensory for users. The distance between the intersection bulb-outs will be 22' apart and mid-block bulb-outs will extend to the same depth. Refer to all all pages that apply- Pp 42-45 and Detailed Block Plans.

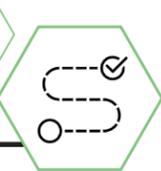
### SECTION



'Columnare' Norway Maple



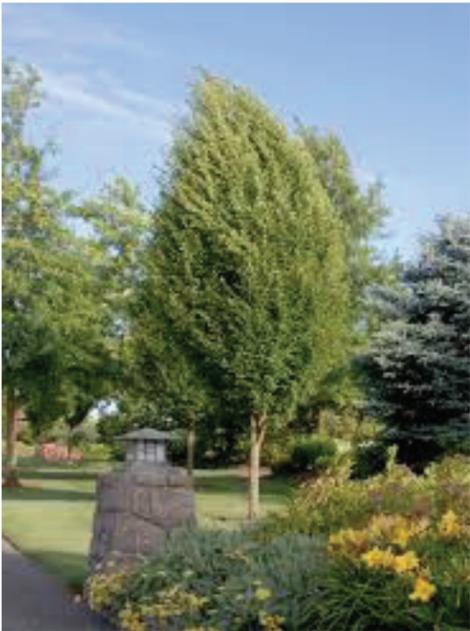
'Corzam' Littleleaf Linden



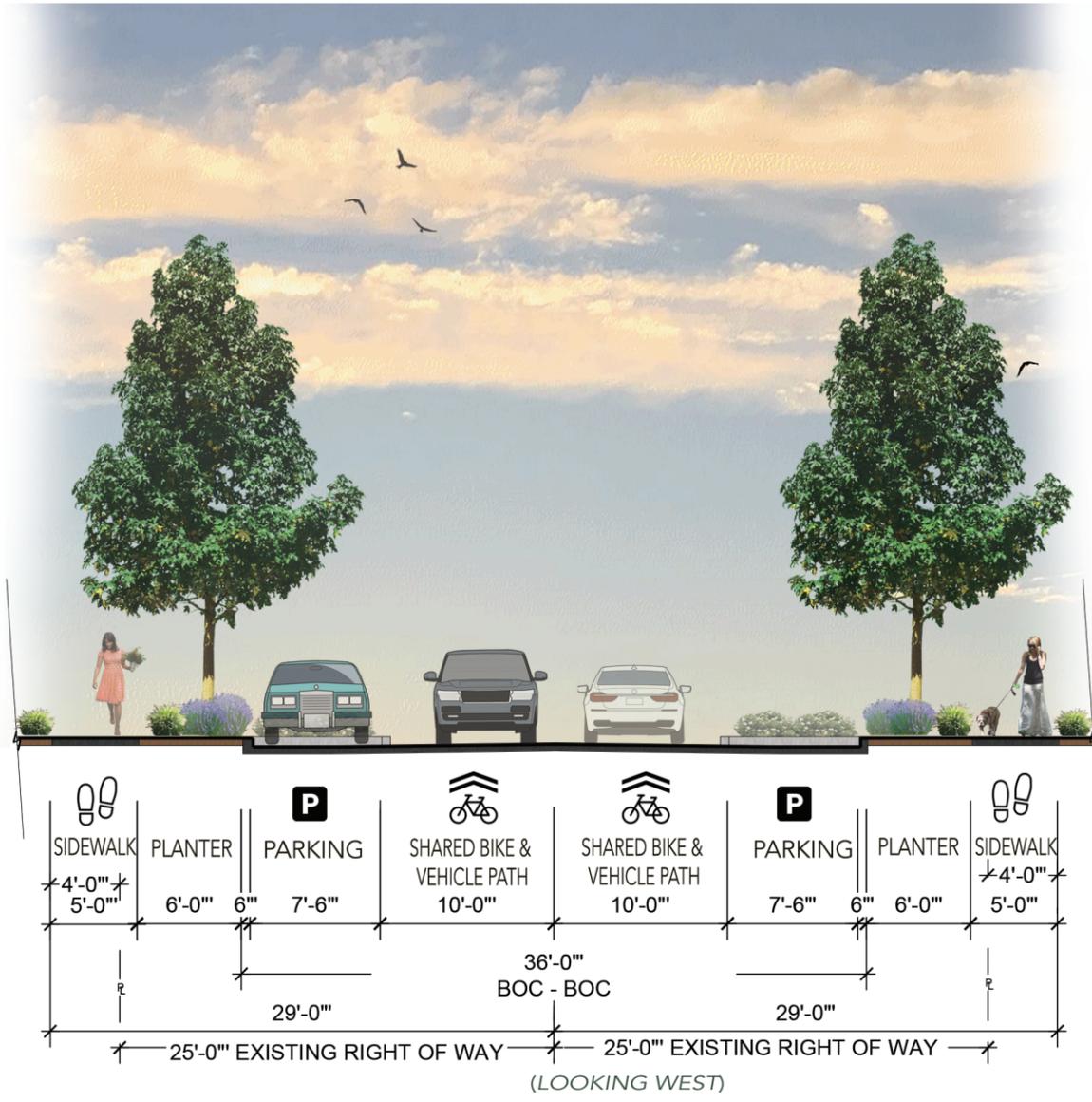
SECTION



'Worpleson' Sweetgum



'Fastigiata' Hornbeam



**GARDEN CITY STANDARD DETAILED STREET SECTION**  
**DETACHED SIDEWALK (CLAY, 35th & 33rd STREET)**

Detached sidewalks are proposed along Clay Street, 33rd Street, and 35th Street. The buffer meets and exceeds code requirements with a 6' planter strip between the sidewalk and curb. These planter strips, which serve site drainage, will have Class II trees at 50' intervals with root barriers and shrubbery at 5 shrubs per 150 square feet, exceeding the minimum of one shrub per 150 square feet, and offering a higher visual interest and many other functions. Adequate 5' sidewalks are provided between the planter strip and building frontages that also provide the more aesthetic higher density plantings that will complement the buildings and frontage spaces. Artistic furnishings such as street lights and bicycles are provided above minimums to add to a strong theme and feel of the development. Refer to all pages that apply- Pp 42-44 for additional information on Streetscape Details.



**STREETSCAPE PLANT PALETTE**

The following Class II and Class I Trees have been selected from the Boise Tree Selection Guide. This guide is a reference for all the trees in the 34th Street Specific Area Plan. In lots that are defined in the Boise River Greenbelt Overlay plant selection will be guided by the Native Plants of the Boise River guide.

**CLASS II TREES**

- Parkway Maple (*Acer platanoides* 'Columnar Broad')
- Common Hackberry (*Celtis occidentalis*)
- Shademaster Honey Locust (*Gleditsia triacanthos inermis* 'Shademaster'™)
- 'Princeton Sentry' Ginkgo (*Ginkgo biloba* 'Princeton Sentry')
- 'Columnare' Norway Maple (*Acer platanoides* 'Columnare')
- 'Corzam' Littleleaf Linden (*Tilia cordata* 'Corzam')
- 'Worplesdon' Sweetgum (*Liquidambar styraciflua* 'Worplesdon')
- 'Fastigiata' Hornbeam (*Carpinus betulus* 'Fastigiata')
- Chanticleer Pear (*Pyrus Calleryana* 'Chanticleer')
- Autumn Purple Ash (*Fraxinus pennsylvanica* 'Autumn Purple')

**CLASS I TREES**

- Flame Amur Maple (*Acer ginnala* 'Flame')
- Columnar Sweet Gum (*Liquidambar styraciflua* 'Slender Silhouette')
- Crimson Pointe Flowering Plum (*Prunus x cerasifera* 'Cripoizam')
- Staghorn Sumac (*Rhus typhina* 'Laciniata')
- Redbud (*Cercis canadensis*)
- Japanese Tree Lilac (*Syringa reticulata*)

**EVERGREEN TREES**

- Skyrocket Juniper (*Juniperus scopulorum* 'Skyrocket')
- Columnar Norway Spruce (*Picea abies* 'Cupressina')
- Brun's Serbian Spruce (*Picea omorika* 'Brun's')

**DECIDUOUS SHRUBS**

- Blue Mist Shrub (*Caryopteris x clandonensis* 'Blue Mist')
- Rubber Rabbitbrush (*Chrysothamnus nauseosus*)
- Blizzard Mockorange (*Philadelphus lewisii* 'Blizzard')
- 'Iceberg' Rose (*Rosa* spp. 'Iceberg')
- Burkwood Viburnum (*Viburnum x burkwoodii* 'Anne Russell')



PLANT SCHEDULE			
TREES	CODE	BOTANICAL NAME / COMMON NAME	REMARKS
	FRA AU2	Fraxinus pennsylvanica 'Autumn Purple' / Autumn Purple Ash	50' H x 35' W, CLASS II
	GLE SHA	Gleditsia triacanthos inermis 'Shademaster' TM / Shademaster Locust	35' H X 35' W, CLASS II, DROUGHT TOLERANT
	PYR PEA	Pyrus calleryana 'Chanticleer' / Chanticleer Callery Pear	30' H X 15' W, CLASS I
SHRUBS	CODE	BOTANICAL NAME / COMMON NAME	REMARKS
	ART POW	Artemisia x 'Silver Mound' / Powis Castle Artemisia	2'-3' H X 2'-3' W, DROUGHT TOLERANT
	BUX WI2	Buxus microphylla japonica 'Winter Gem' / Winter Gem Boxwood	4' H X 4-5' W
	CAL KAR	Calamagrostis x acutiflora 'Karl Foerster' / Feather Reed Grass	5.5' H X 3' W
	CAR CLA	Caryopteris x clandonensis 'Blue Mist' / Blue Mist Shrub	3' H X 3' W, DROUGHT TOLERANT
	HEL SEM	Helictotrichon sempervirens / Blue Oat Grass	2.5' H X 2.5' W, DROUGHT TOLERANT
	IBE SEM	Iberis sempervirens 'Purity' / Candytuft	6" H x 12" W DROUGHT TOLERANT
	ILE SKY	Ilex crenata 'Sky Pencil' / Sky Pencil Japanese Holly	6' - 8' H X 2'-3' W
	LAV HI2	Lavandula angustifolia 'Hidcote Blue' / Hidcote Blue Lavender	2' - 2.5' H X 2' - 2.5' W, DROUGHT TOLERANT
	PEN KA2	Pennisetum alopecuroides 'Karlay Rose' / Fountain Grass	2.5' H X 2.5' W, DROUGHT TOLERANT
	RHU GRO	Rhus aromatica 'Gro-Low' / Gro-Low Fragrant Sumac	2.5' H X 6' + W, DROUGHT TOLERANT
	ROS ADE	Rosa spp 'Iceberg' / 'Iceberg' Rose	3.5' H X 3.5' - 4' W
	SAL NEM	Salvia nemorosa 'East Friesland' / East Friesland Perennial Sage	1.5' H X 2' W, DROUGHT TOLERANT



GRO-LOW FRAGRANT SUMAC



BLUE OAT GRASS



HIDCOTE BLUE LAVENDER



POWIS CASTLE ARTEMISIA



'BLUE SHAG' WHITE PINE

### EVERGREEN SHRUBS

- Carol Mackie Daphne (Daphne x burkwoodii 'Carol Mackie')
- Sky Pencil Japanese Holly (Ilex crenata 'Sky Pencil')
- Creeping Mahonia (Mahonia repens)
- 'Blue Shag' White Pine (Pinus strobus 'Blue Shag')
- Fine Line Buckthorn (Rhamnus frangula 'Fine Line')
- Gro-Low Fragrant Sumac (Rhus aromatica 'Gro-Low')

### GROUND COVERS

- Moonshadow Euonymus (Euonymus fortunei 'Moonshadow' TM)
- Massachusetts Manzanita ('Arctostaphylos uva-ursi 'Massachusetts')
- Blue Spruce Stonecrop (Sedum reflexum 'Blue Spruce')
- Dragon's Blood Stonecrop (Sedum spurium 'Dragon's Blood')
- Golden Creeping Jenny (Lysimachia nummularia 'Aurea')

### PERENNIALS & VINES

- Powis Castle Artemisia (Artemisia x 'Silver Mound')
- Hidcote Blue Lavender (Lavandula angustifolia 'Hidcote Blue')
- 'Candytuft (Iberis sempervirens 'Purity')
- East Friesland Perennial Sage (Salvia nemorosa 'East Friesland')
- Zagreb Coreopsis (Coreopsis verticillata 'Zagreb')
- Red Hot Poker (Kniphofia uvaria 'Echo Rojo')
- Arizona Apricot Blanket Flower (Gaillardia aristata 'Arizona Apricot')
- Magnus Purple Coneflower (Echinachaea pupurea 'Magnus')
- Clematis (Clematis x 'Niobe')
- Clematis (Clematis x 'The President')
- Virginia Creeper (Parthenocissus quinquefolia englemannii)

### ORNAMENTAL GRASSES

- Blue Oat Grass (Helictotrichon sempervirens)
- Burgundy Bunny Dwarf Fountain Grass (Pennisetum alopecuroides 'Burgundy Bunny')
- 'Karlay Rose' Fountain Grass (Pennisetum alopecuroides 'Karlay Rose')
- Feather Reed Grass (Calamagrostis x acutiflora 'Karl Foerster')

**STREETSCAPE FURNISHINGS**

A pallette of street furnishings have been selected to create unified character in the streetscape fabric. These furnishings include benches, a variety of bike racks, raised planters for vegetables and trash receptacles. The images to the right are conceptual examples that characterize the streetscape furnishings.

**BENCHES**

Benches along the streetscape shall be locally sourced and manufactured sandstone slabs cut to 18" height with a smooth top. The following images represent the natural look & feel of custom sandstone benches.

**BIKE RACKS**

Two type of bike racks have been chosen for the streetscape. Artful, locally designed bike racks shall be used along sidewalks and bulb out areas. The creation of these bike racks will create opportunities for local artisans to express the local identity. Simple and sculptural Landscape Forms Loop bike racks shall be used in the more screened areas including the cut outs along the raised building terraces on 34th Street.

**TRASH RECEPTACLES**

Trash receptacles will be located along attached and detached sidewalks to promote a clean streetscape. Simple, durable and easy to maintain Forms and Surfaces Urban Renaissance trash receptacles have been selected for the streetscape.

**RAISED STEEL VEGETABLE PLANTERS**

Raised vegetable planters will be placed along attached sidewalk streets on the sidewalk next to parked cars. These planters should have staggered heights with patina-style steel and a 2' wide maximum width. The fabrication of these would be a great opportunity for local artisans. It is important to fold the top edges over for public safety.



*Custom sandstone bench with smooth top*



*Custom bike rack example*



*Raised steel planters for vegetables*



*Sandstone bench with natural feel*



*Custom bike rack example*



*Raised steel planters with folded edge*



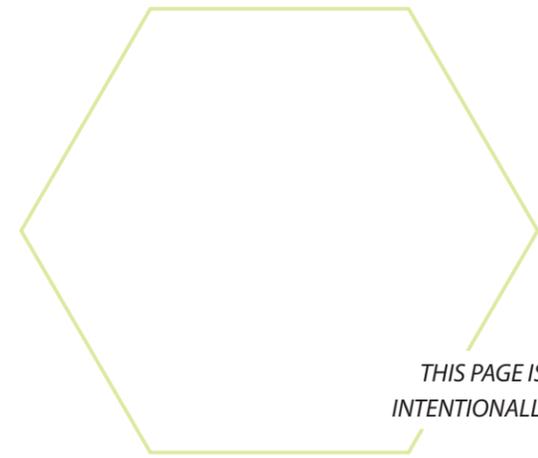
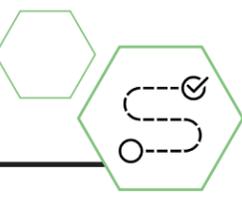
*Landscape Forms Loop bike rack*



*Forms & Surface Urban Renaissance trash can*



*Raised planters with staggered heights & widths*

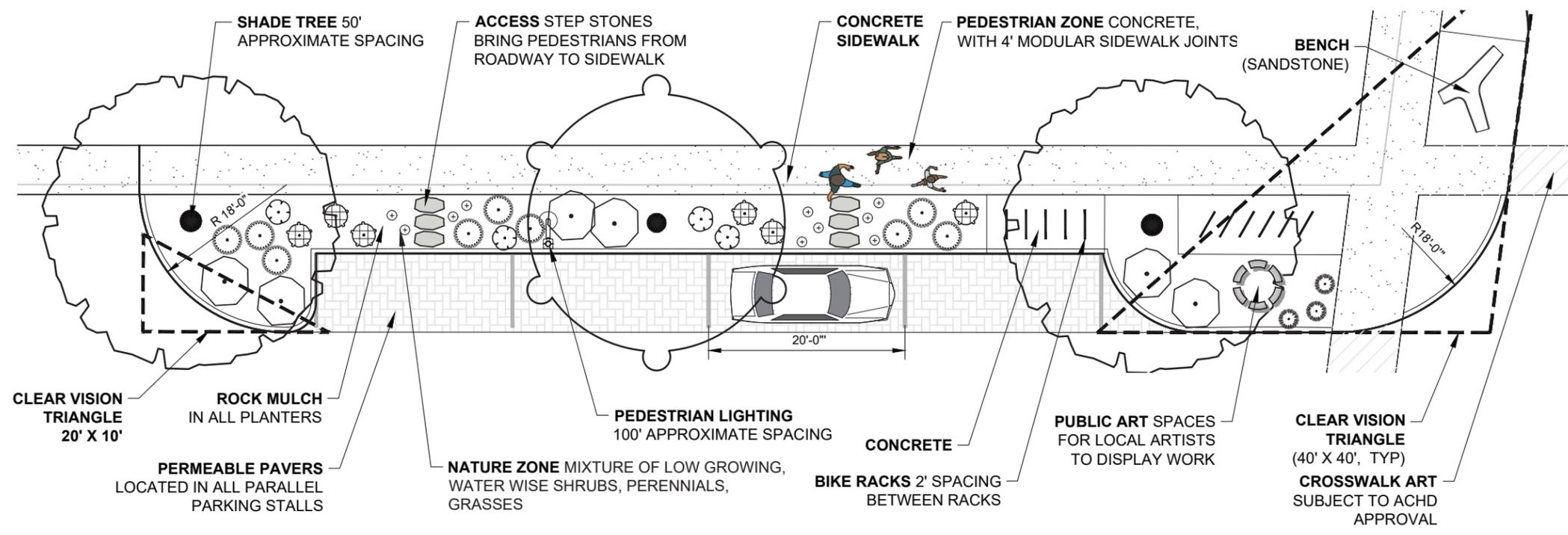


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**STREETSCAPE  
DETAIL PLANS**

Streetscape detail plans show the exact placement and type of the many features that are proposed to maximize the user experience. The plans show the differences of the detached and attached sidewalks in relation to the building frontages, street, and various planter and furnishing locations. Depicted are the trees spaced at 50' intervals coupled with the various types of shrubs and other ground plane plantings. Stepstones are shown to allow those who parallel park easy access from their car onto the sidewalk. And the various furnishings such as bike racks and light poles are also highlighted and how they relate to the site layout. The elevation drawings show the sidewalk transition at the raised terraces at building frontage.

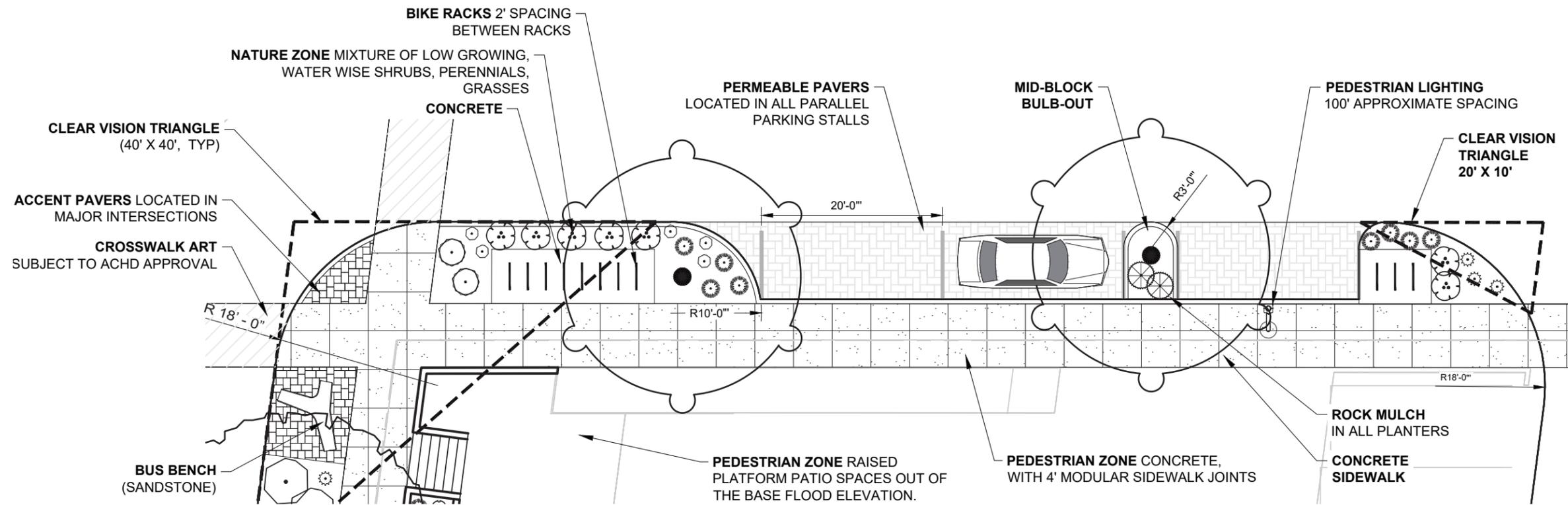
**DETACHED SIDEWALKS**



*\* Pedestrian Zone refers to the streetscape area where pedestrian and ADA access is permitted.*



ATTACHED SIDEWALKS

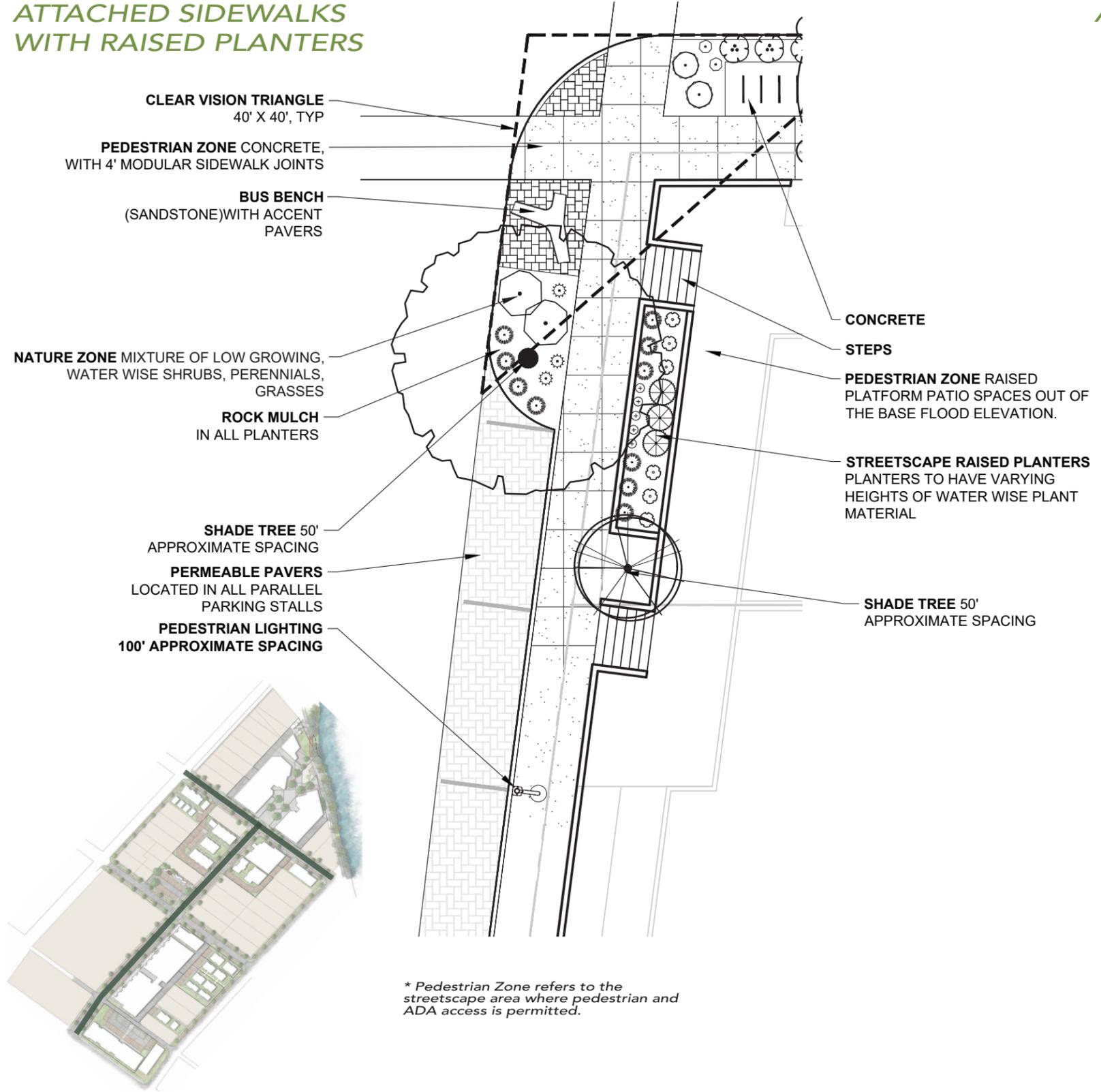


\* Pedestrian Zone refers to the streetscape area where pedestrian and ADA access is permitted.



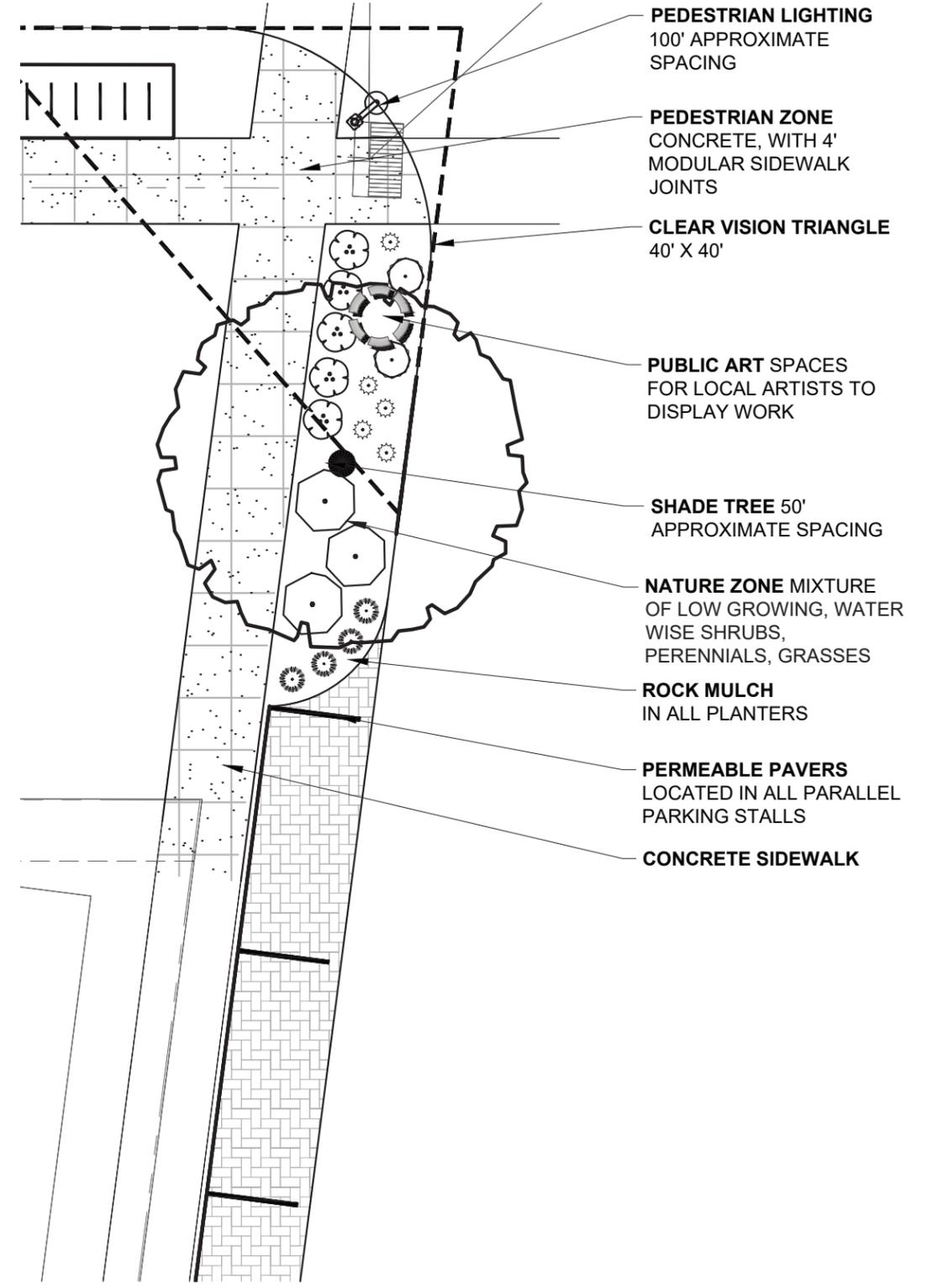


### ATTACHED SIDEWALKS WITH RAISED PLANTERS

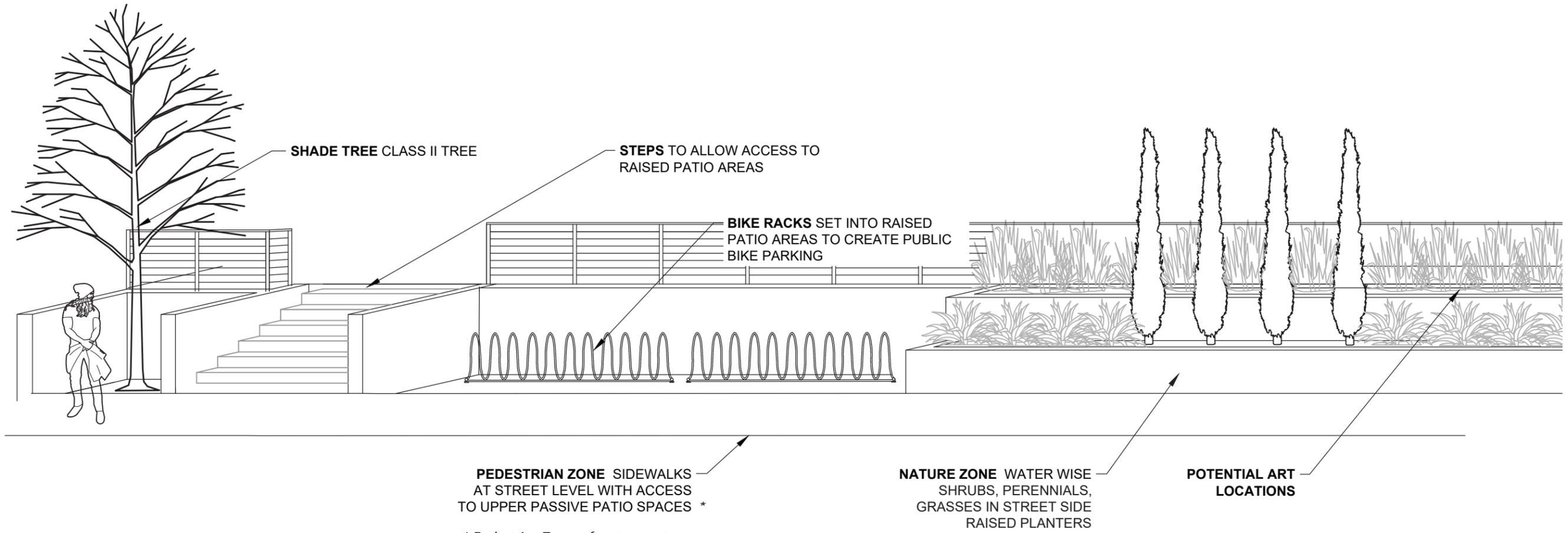


\* Pedestrian Zone refers to the streetscape area where pedestrian and ADA access is permitted.

### ATTACHED SIDEWALKS BULB-OUTS



**ATTACHED SIDEWALKS BULB-OUTS**



*\* Pedestrian Zone refers to streetscape area where pedestrian and ADA Access is permitted.*



*Crosswalk art per ACHD approval*

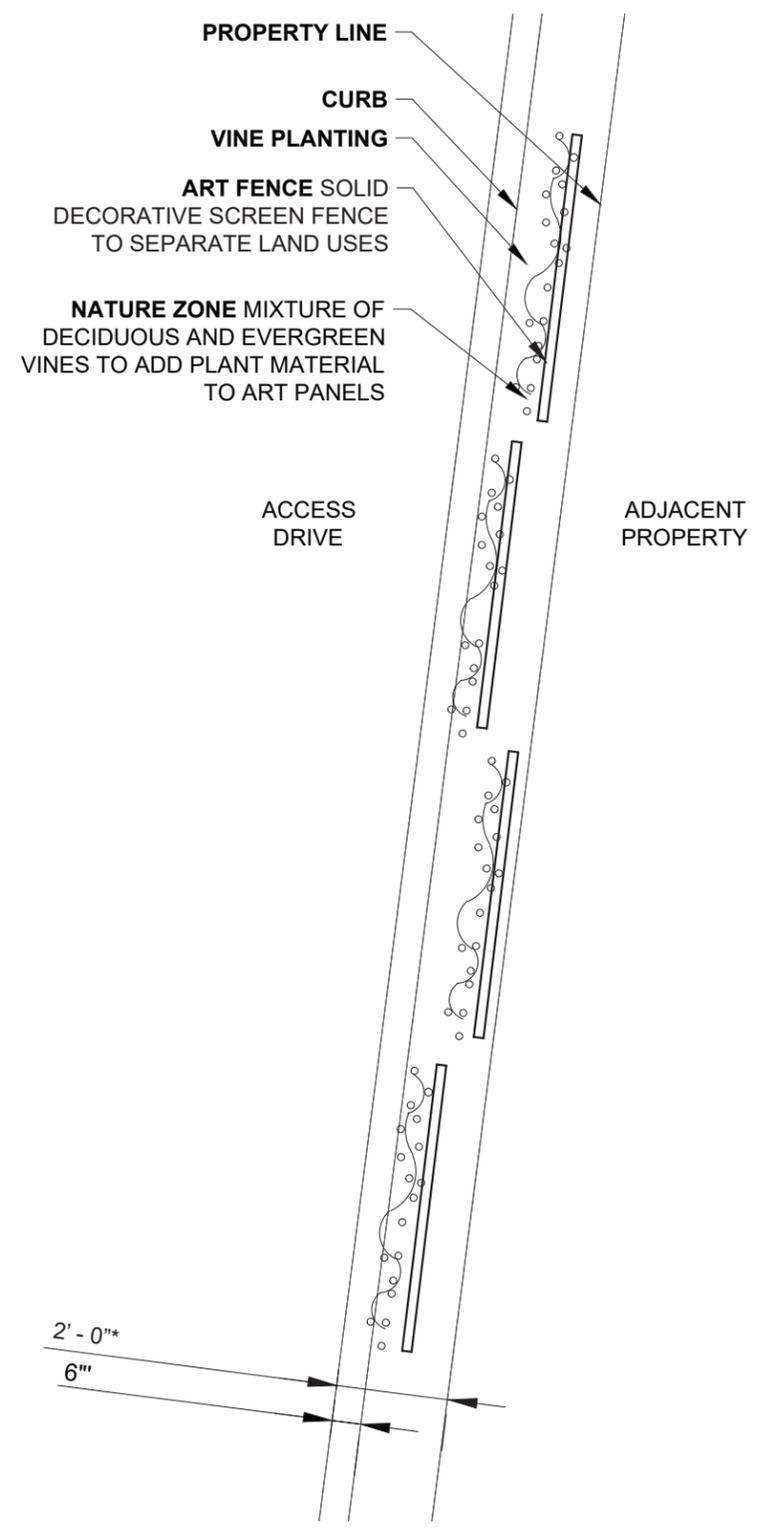


*Raised patio & planting example*

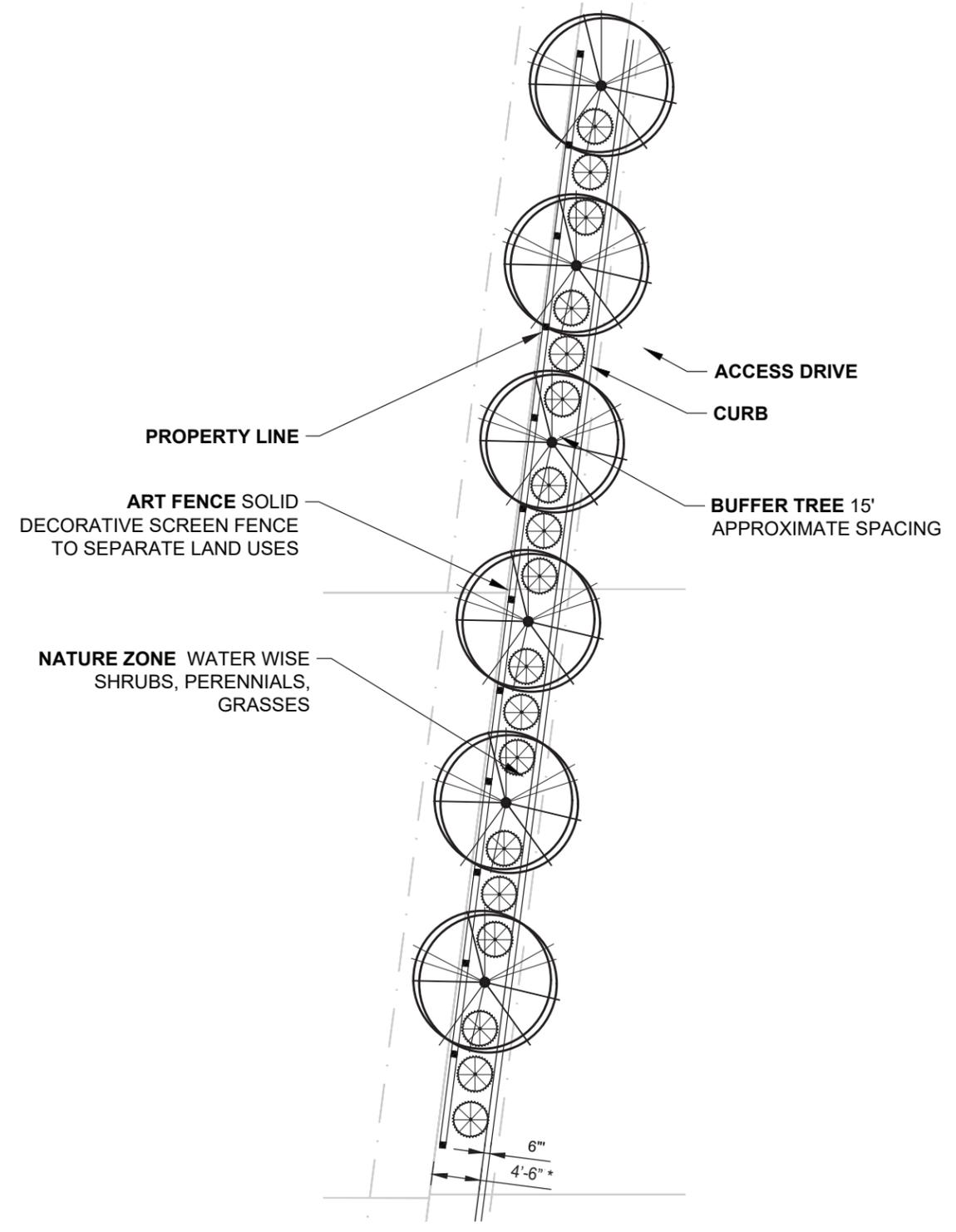


*Bike racks*

**PERIMETER -MINIMUM WIDTH**



**PERIMETER -MAXIMUM WIDTH**



\* Noted setbacks are the preferred dimensions that will be drawn in Construction Documents



Art fence & planting example



Art fence example



Art fence & planting example



Art fence & planting example

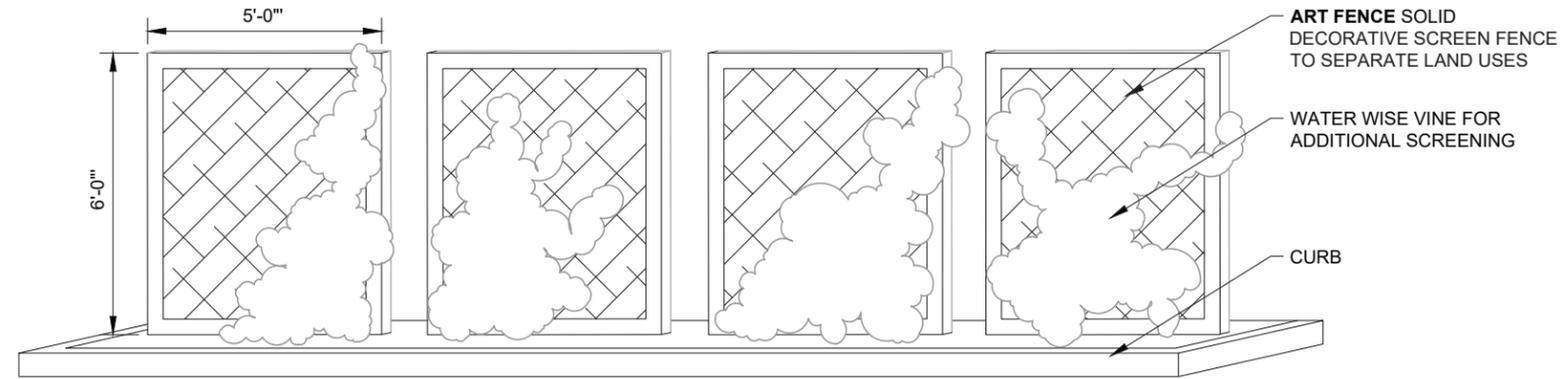


Art fence example



Art fence example

PERIMETER -MINIMUM WIDTH ELEVATION



The perimeter plans show how the mixed-use and live/work areas will be buffered from the surrounding non-participating lots. There are a maximum and minimum sizes that offer similar yet different elements. The minimum perimeter widths show how screening and buffering will be provided through the use of screen features and vines and plantings to soften and provide visual aesthetic, where trees cannot be located due to planter width. The maximum planter widths are illustrated to show the trees in addition to plantings and vertical art features that help screen and offer aesthetics. The elevations pictured at the right represent examples of the vertical design of the screen elements would look like.

PERIMETER -MAXIMUM WIDTH ELEVATION



# LANDSCAPE GUIDELINES

**Purpose & Goals:**  
 Support local water conservation by using adaptive or native low-water use/ water-wise plants  
 Enhance 34th Street neighborhood aesthetics  
 Promote Safety and other functional benefits from plants  
 Establish Identity and Sense of Place in the 34th Street neighborhood

The images shown are conceptual examples of the landscape elements listed below.

### Streetscape Planting Buffer

**Attached sidewalks:**  
 1 Class II Tree per 50 Lineal Feet (Per Garden City Code)  
 5 Shrubs per 150 SF (vs. 1 shrub per 150 SF GC Code) in Bulb Outs & Raised Planters.

### Streetscape Raised Planters:

Class II Street trees may be located in the planters. The maximum Tree canopy width at maturity must remain clear of building walls and overhangs.  
 5 Shrubs per 150 SF in planters (above code minimums).  
 Suggested trees for raised planters include:  
 Parkway Maple (Acer platanoides 'Columnar Broad') (40' H X 25' W)  
 'Columnare' Norway Maple (Acer platanoides 'Columnare') (40' H X 15' W)  
 'Princeton Sentry' Ginkgo (Ginkgo biloba 'Princeton Sentry') (45' H X 25' W)  
 'Fastigiata' Hornbeam (Carpinus betulus 'Fastigiata') (35' H X 25' W)  
 Chanticleer Pear (Pyrus Calleryana 'Chanticleer') (30' H X 15' W)  
 'Worplesdon' Sweetgum (Liquidambar styraciflua 'Worplesdon') (40' H X 25')

Planter Wall materials should be any of the following materials:

1. Wood
2. Steel
3. Concrete
4. Stone



Detached sidewalks

the site.  
**Detached Sidewalks:**  
 1 Class II Tree per 50 Lineal Feet (Per Garden City Code)  
 5 Shrubs per 150 SF (vs. 1 shrub per 150 SF GC Code) in Bulb Outs & Parkways between Parking and sidewalks.  
 Provide Access Step stones between Parking and sidewalks every 2-3 parking spaces at necessary locations.

### Perimeter Planting:

**Description:** 2' Wide minimum perimeter areas between participating & adjacent properties  
 Integrate 6' high grasses, vines or shrubs with 6' high decorative screen fence elements. Decorative screen fence elements should be any of the following materials:  
 1. Wood  
 2. Steel  
 3. Concrete  
 Decorative screen fence material should match materials of building on the site.  
 4. Stone  
**Townhouse:**  
 1 Class II tree per unit  
 5 shrubs per 150 SF

**Parking Lot (Interior Parking lot)**  
 1 tree per 20 Spaces (or 1 Tree per 1000 SF)(Per Reed Street)

**Tree Species, Sizes, Biodiversity**  
 Refer to Treasure Valley Tree selection Guide for Tree Species. Class II and I Trees, Shrubs, Ground covers, Perennials and Ornamental Grasses should be Selected from the 34th Street Plant Palette. Planting within lots that are defined by the Boise River Greenbelt Overlay will be selected from the Native Plants of the Boise River guide.  
 The minimum plant sizes shall follow the requirements provided by Garden City Municipal Code (8-41-3-E)



Decorative screen fence

Class II Shade Trees - 2" Min. caliper  
 Class I -Ornamental Trees- 2" min. Caliper  
 Evergreen trees- 6' H minimum  
 Woody Shrubs - 2 gallon container minimum  
 Tree Species Mix (Garden City & Boise)

A variety of trees shall be used when 5 or more trees are planted to satisfy requirements. The number of different tree species should vary according to the following table from Garden City Municipal Code:

Required Number Of Class II Or Class III Trees	Minimum Number Of Species
5 to 10	2
11-30	3
31-50	4
More than 50	5

(Garden City Municipal Code - 8-41-3- G.)(Ord. 944-12, 5-14-2012)

### Mulch

Mulch shall be applied in all planting areas unless noted otherwise. Rock mulch shall be applied at a depth of 3" over commercial-grade weed barrier fabric, if approved bark mulch shall be installed over soil, apply commercial grade pre-emergent. Natural-colored gravel rock mulch is encouraged as a mulch material.

### Bike Parking

All bike racks shall be spaced at 2' apart in designated bike parking areas along sidewalks, within lots and bike parking areas along building frontage on 34th Street. Install per spec or manufacturer's recommendations



Decorative screen fence

Artful, locally designed and fabricated bike racks shall be located in the most visible locations such as along streets and in bulb-outs near intersections.  
 Lawn is not proposed in any locations. Some areas will provide high quality artificial turf that will not require maintenance, but provides the exact visual appeal of lawn and usability.

Irrigation will be provided in all landscape areas in a subsurface fashion with either drip emitters or small pop-ups in necessary locations such as garden beds. The irrigation system will be designed to efficiently promote the necessary volume of water to ensure plant viability pertaining to all components in type and size depending on the available static pressure of the water source.

Clear Vision triangles are depicted at all intersection and driveway/alley way ingress & egress locations. No plantings or other features shall be over 36" to ensure the safety of pedestrians, cyclists, and vehicle users.

Street Lights shall be provided along all street improvements. Each intersection shall have a minimum of one overhead vehicle rated street light to promote safety for cars, cyclists, and pedestrians, and all these lights shall match. Pedestrian street lights shall be located along all street frontages and shall be selected from the approved options. The specific locations of streetlights will be reviewed and integrated with the design at the time of constructions drawings. Along each individual street all pedestrian lights shall match for continuity, each street may have an option different from another.



Streetscape planting



*Streetscape planting*



*Decorative screen fences & perimeter planting*



*Raised planters*



*Artful, custom bike racks*



*Decorative screen fences*



*Streetscape planting*



*Raised planter walls on attached sidewalks*



*Access step stones on detached sidewalks*



*Raised planters*



*Raised planters*

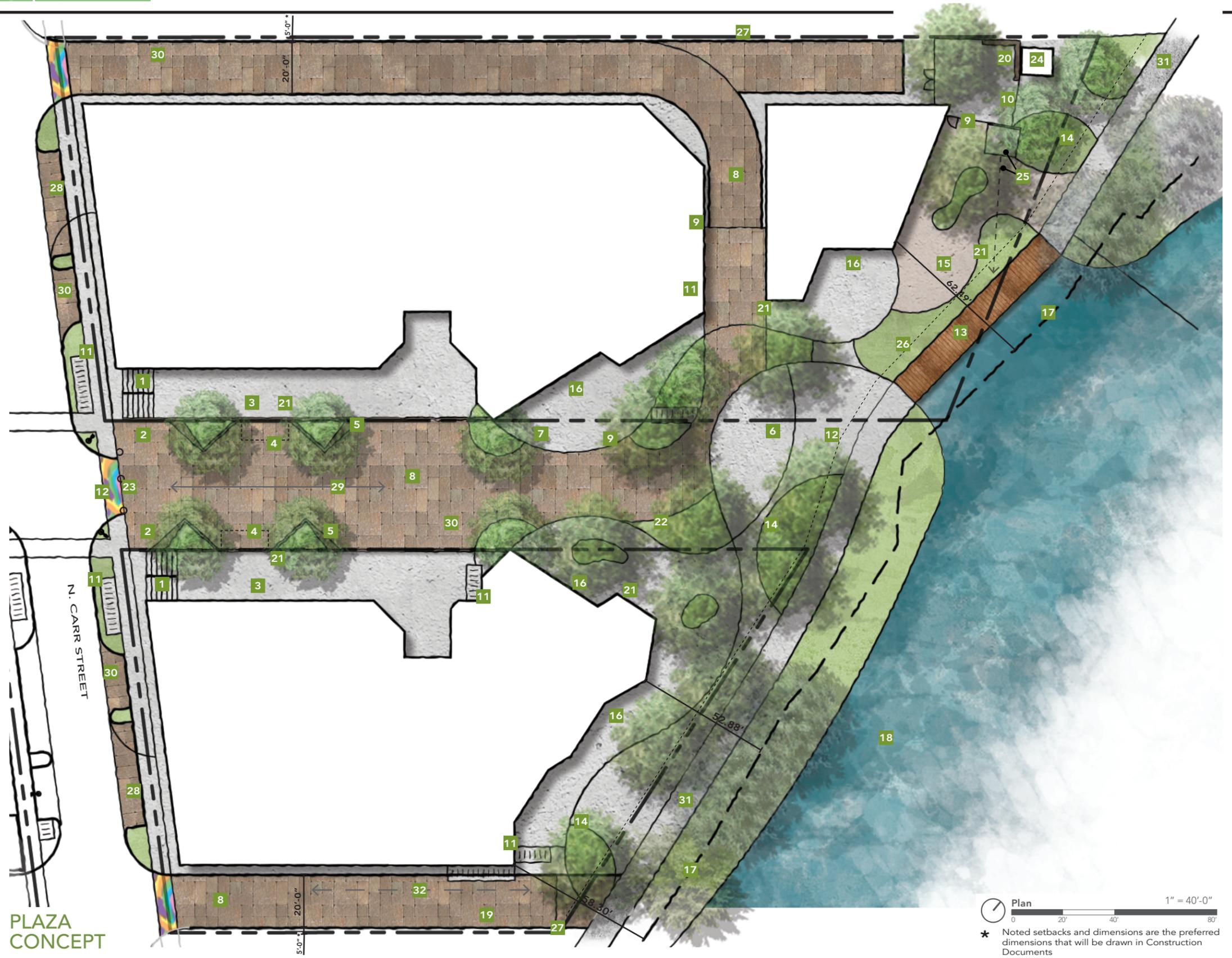


*Raised planters*



*Street trees*

- Key**
- 1 Steps
  - 2 Entry columns
  - 3 Raised patio at plaza level
  - 4 Kiosk areas
  - 5 Irregular seat bench planters
  - 6 Paver interest
  - 7 Deciduous trees
  - 8 Fire access drive
  - 9 Gate location
  - 10 Surfer's Alley
  - 11 Bike Racks
  - 12 Pedestrian/ bike dismount area
  - 13 Existing Bridge
  - 14 Planters with native, drought-tolerant plantings
  - 15 Crushed granite
  - 16 River activated patio
  - 17 Ordinary High Water Mark at 6500 cfs
  - 18 Boise River
  - 19 Pet Park
  - 20 Surfers locker area
  - 21 Community garden boxes
  - 22 Children's natural play area
  - 23 Bollards
  - 24 Irrigation Control Building
  - 25 Irrigation Control Structure & Pipe
  - 26 Greenbelt Easement
  - 27 Perimeter property line
  - 28 Underground parking entry
  - 29 Ramp location
  - 30 Permeable pavers
  - 31 Existing greenbelt
  - 32 Bike zip path



Plan 1" = 40'-0"

\* Noted setbacks and dimensions are the preferred dimensions that will be drawn in Construction Documents



## DETAILED LANDSCAPE PLANS

### THE EDDY PLAZA

Amenities are essential to the quality of life for multi-family developments. Garden City promotes and requires amenities of different types and quantities throughout different sizes of developments

The 34th Street Specific Area Plan mixed-use development area immediately adjacent to the greenbelt and river, which provides 100 residential units, meets and exceeds the minimum requirements Garden City has outlined. The city requires three minimum amenities from each category, which are provided including; Eddy Plaza, Natural Children's Play Area, roof yard game areas, community garden boxes, and a Pet Park. Other elements that will be available to residents and consumers alike include public art, events, goods and services, all coupled with the river and greenbelt as highlighted below.

The Eddy Plaza is a significant space in the 34th Street Specific Area Plan, it's envisioned to be a dynamic river activated public gathering place which integrates an utmost vital resource, the Boise River. It acts as a catalyst between the built and natural environment, allowing people to gather near the greenbelt and river & white water park, while taking advantage of the many goods and services provided in the river activated frontage of the mixed-use buildings. This node will serve the community strongly and draw people from surrounding populations, offering markets, music & events, patio spaces, kids play areas, with various materials and elements that create a natural and comfortable feeling. Adults, kids, and pets will enjoy passing time with the many features provided, such as vendors, events, sculptural art, planters with seat benches, and other aesthetic

elements designated throughout the many spaces that make up the plaza, while also enjoying the benefits that a river corridor offers. A unique space, Surfer Amenity area with lockers and surfboard racks provides a place for those taking advantage of the whitewater park, resting, storage and gathering. Fire access needed a creative solution, this was successfully achieved by making vehicle access a place, rather than being designed around the emergency access vehicle. Materials that offer a natural and real feeling enhance the river frontage by use of granite, boulders, aesthetically pleasing pavers that will enhance the 'pop' of artistic elements such as artistic light columns. This space aims to bring the river life and mixed-use/ market life together and promote a fun diversity in options for users from the community and surrounding areas, and will be easily accessible from the greenbelt and 34th/Carr Streets. Unique to this space is the Boise River Greenbelt Ordinance which requires 50% deciduous tree canopy cover in the setback bordering the river. This requirement aims to benefit the ecology of the river and riparian zone which is vital, and in tune with 34th Street Specific Area Plan vision. The trees in this plan are represented at more than 50% of their full width at maturity. These trees will be planted amongst native, drought-tolerant plants in ground-level planter beds throughout the plaza. Thoughtful consideration and critical thinking was crucial to providing an area that serves the river & whitewater park, greenbelt, and local commerce & residential areas, that so many near and far will know as Eddy Plaza.



Crushed granite plaza



Drought-tolerant, native plantings



Seat bench planters



River activated frontage



River activated frontage



Entry columns



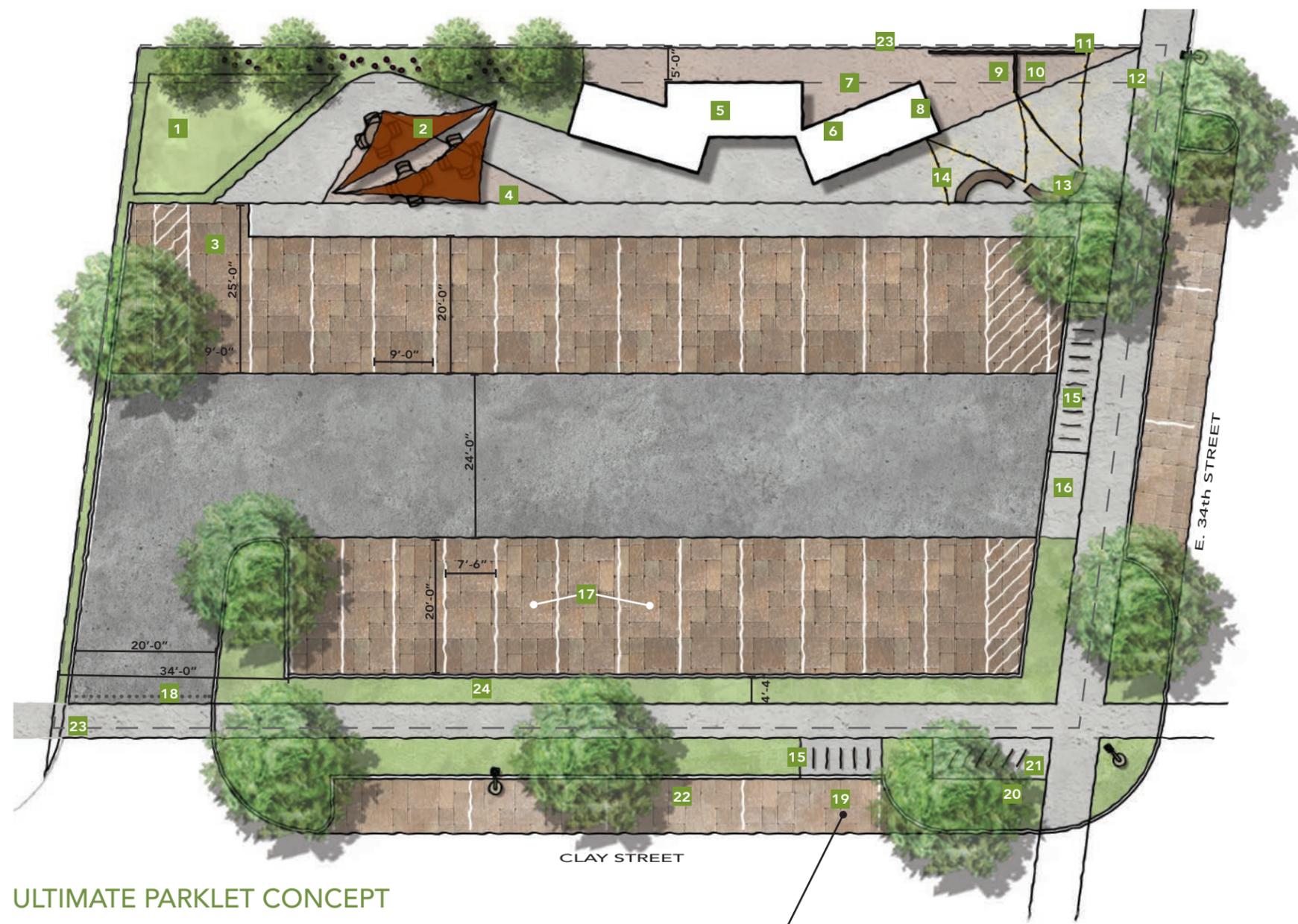
Public gathering place



Market and community event space

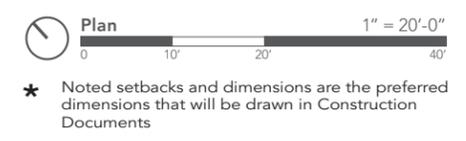
**Key**

- 1 Dog park area
- 2 Tailgate area with moveable furniture and shade sails
- 3 'Bells for Books' parking space (25' Long)
- 4 Crushed granite
- 5 Micro food space
- 6 Restroom & changing area with access from front
- 7 Easement
- 8 Shower area at back
- 9 Lockers
- 10 Stand up paddle board racks
- 11 Art mural location
- 12 Entry to surfers alley
- 13 Sandstone benches
- 14 Overhead string lights
- 15 Bike parking
- 16 Walkway & E-Scooter parking
- 17 Electrical car parking/charging spaces(3)
- 18 Temporary chain location for events
- 19 Garden City Police parking space
- 20 Art
- 21 Boise Green Bike Hub
- 22 Uber/Lift Ride Hailing Location
- 23 Property Line
- 24 Planter strip with shrubs, perennials and groundcovers from 34th Street Plant Palette



**ULTIMATE PARKLET CONCEPT**

**Garden City Police Parking space**  
 We show a parking space dedicated to the Garden City Police Department, our preference would be to have this space be exclusive to the GCPD. We want to continue the community connection with local law enforcement, we want to honor all the work of the GCPD for decades in our community. This space location was designed for easy access, and for its centralized location to the East End.



Sandstone benches



Outdoor paddle board rack



'Bells for Books' mobile library



String lights



Showers



## DETAILED LANDSCAPE PLANS THE ULTIMATE PARKLET

As we welcome visitors to the Live Work Create District the Ultimate Parklet provides more than just a parking lot. With signage to welcome visitors, river recreation users, residents and guests, the first stop if by car is a centralized location we call our Ultimate Parklet.

Here you can park, enjoy semi public amenities such as a restroom, outdoor lockers, changing area, pet park space, water fountains, bicycle service stations. From here we recommend exploring our neighborhood by foot or bike, there is so much to do and see from the pedestrian experience!

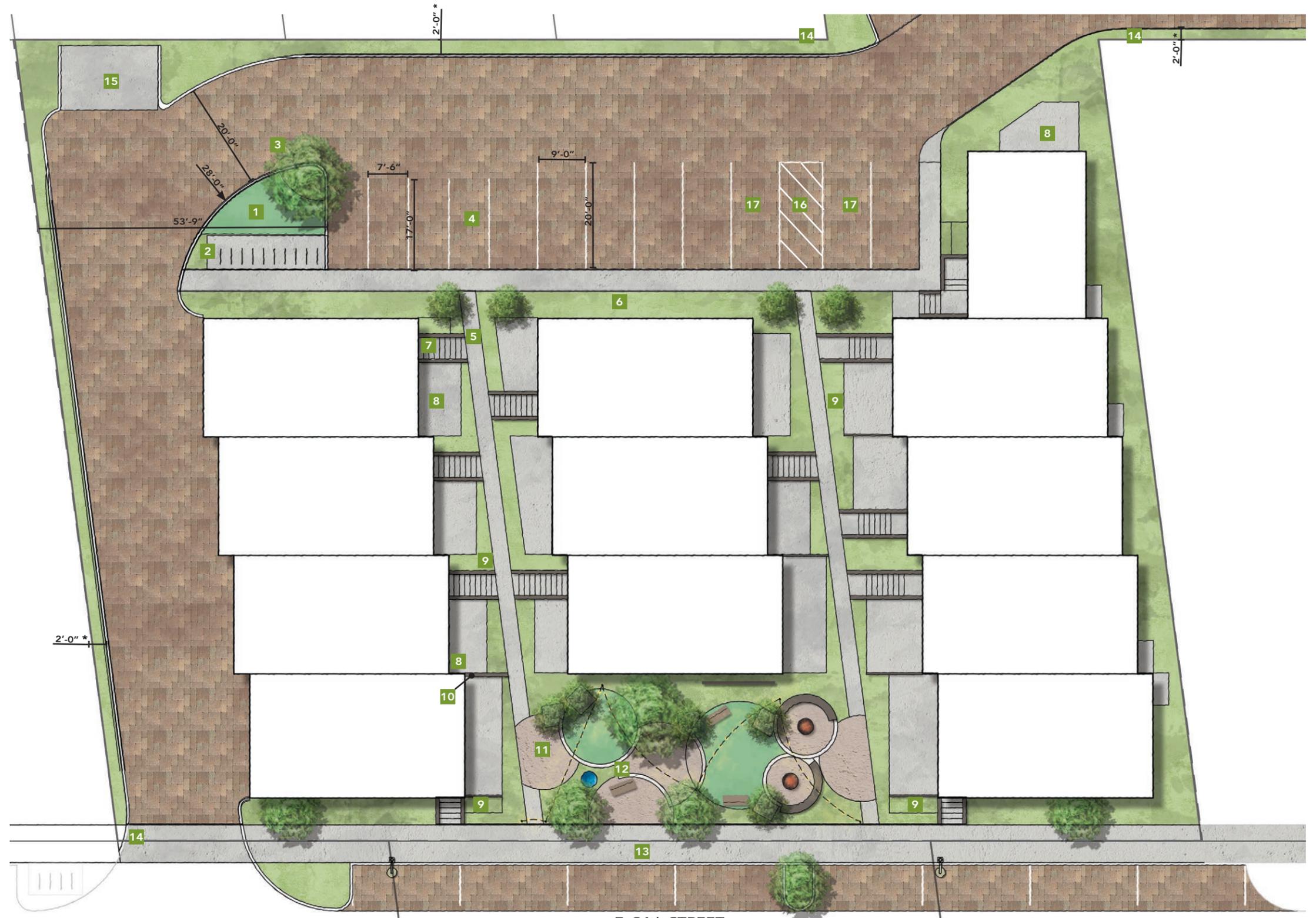
The single entrance also serves another purpose! The Parklet can be easily closed for community interaction. Imagine having a "kids Fair" and having all the amenities as a park in an urban environment. Our parklet can be easily transformed into a park to facilitate neighborhood gatherings, community interactions.

We have provided 1 oversized space for large vehicles such as the Mobile library "Bells for Books" to provide them a space in the live, work, create community.

**DETAILED LANDSCAPE PLANS**  
**YARDBIRD ARTISAN**  
**GATHERING PLACE**

The Artisan Gathering Place area promotes a unique experience, that provides a great public/private oriented gathering space. The central gathering area is distinguished by an open sidewalk accessible feeling from the private patios which are tucked away and elevated with more direct pathways. The space provides seating around fire pits, a water feature, various surface materials from granite to turf, and overhead string lighting that adds comfort. Many of the features are mobile, such as seating, so flexibility allows for better experiences. Users can easily access the space to pass time with friends, and residents have a retreat for more immersed gathering from their private patios.

- Key**
- 1 Pet park with artificial turf
  - 2 Bike racks (11 total)
  - 3 Deciduous tree
  - 4 Private parking stalls
  - 5 Walkway
  - 6 Foundation planting
  - 7 Steps
  - 8 Patio
  - 9 Raised Planters
  - 10 Art screens
  - 11 Paver interest
  - 12 Common area
  - 13 Sidewalk
  - 14 Property Line
  - 15 Trash Enclosure
  - 16 ADA accessible aisle
  - 17 ADA accesible parking stall



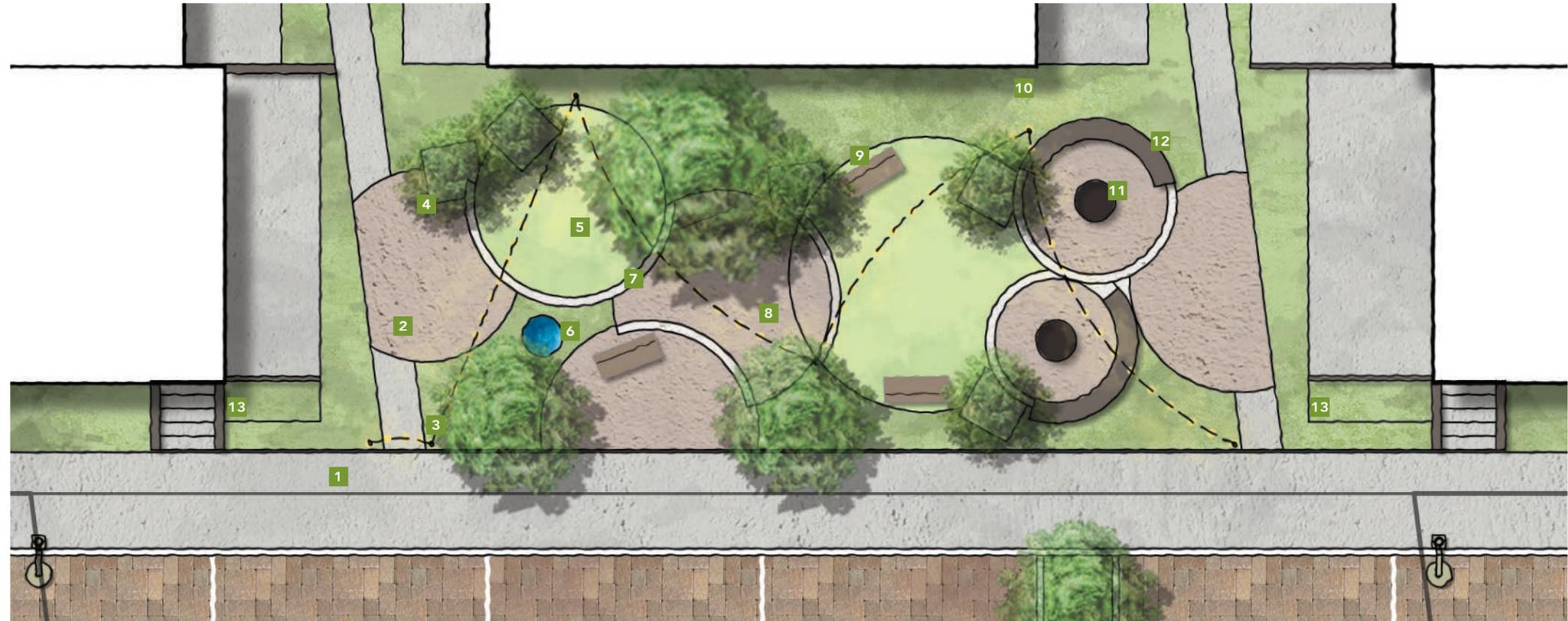
**ARTISAN GATHERING PLACE CONCEPT**

E. 34th STREET

Plan 1" = 20'-0"

0 10' 20' 40'

\* Noted setbacks and dimensions are the preferred dimensions that will be drawn in Construction Documents



- Key**
- 1 Sidewalk
  - 2 Walkway
  - 3 String lights
  - 4 Planter boxes with trees
  - 5 Artificial turf
  - 6 Stone water feature
  - 7 Paver bands
  - 8 Crushed granite
  - 9 Moveable seating
  - 10 Art wall
  - 11 Gas fire pit
  - 12 Sandstone benches
  - 13 Raised planter

ARTISAN GATHERING PLACE COMMON AREA



Fire pit



Water feature



String lights



Crushed granite with paver bands



Art wall

# DETAILED LANDSCAPE PLANS YARDBIRD ARTISAN GATHERING AREA

The theme of the Yardbird Artisan Gathering Place offers more of a private sense for residents, but with comfort in access and passing through from the sidewalk and parking areas. Private patios are delineated by planters with artistic materials that help distinguish resident space from the more public pathway and pet park. The patios are also elevated which further enhance enjoyment, and delineation from private to public.

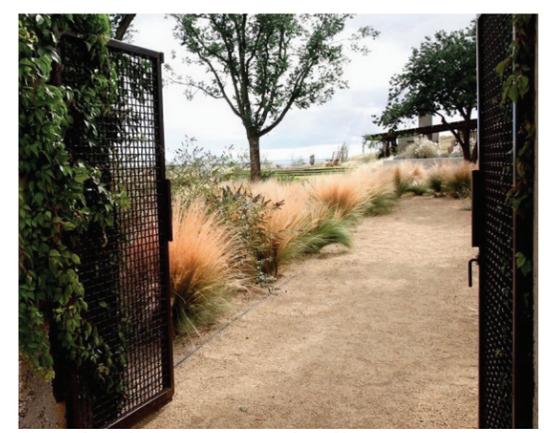
- Key**
- 1 Sidewalk
  - 2 Raised planter
  - 3 Steps
  - 4 Deciduous tree
  - 5 Gate entry
  - 6 Crushed granite pathway
  - 7 Concrete entry
  - 8 Paver pathway
  - 9 Foundation planting
  - 10 Patio
  - 11 Pet park with artificial turf
  - 12 Trash bin & bag station
  - 13 Screen wall and planting
  - 14 Property line
  - 15 ADA accessible parking stall
  - 16 ADA accessible aisle
  - 17 Garage (3)



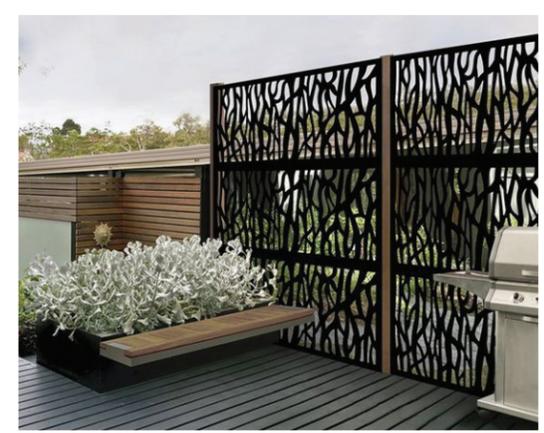
ARTISAN GATHERING PLACE CONCEPT



Raised planters



Crushed granite path alley



Screen wall



Art fence



Art panel screens



Raised planters



Live-work unit



Planter boxes & moveable seating



Raised planters



Live work unit entrances

**Key**

- 1 Walkway
- 2 Live/Work/Townhomes
- 3 Small patio
- 4 Concrete stepping stones
- 5 Live/Work Townhomes/  
Exhibition House
- 6 Flexible patio space
- 7 Sitting area with fire pit
- 8 Artful, custom bike racks
- 9 Surel Michell Honorary Wall  
location
- 10 Existing tree
- 11 Crushed granite pathway
- 12 Stepping stones
- 13 Small patio
- 14 Common area with crushed  
granite
- 15 Entry
- 16 Steps
- 17 Private parking
- 18 Deciduous tree
- 19 Property line
- 20 Gravitas
- 21 Open ditch to be  
community amenity feature
- 22 Intersection bulb-out
- 23 ADA accessible aisle
- 24 ADA accessible stall



**ART BLOCKS CONCEPT**



\* Noted setbacks and dimensions are the preferred dimensions that will be drawn in Construction Documents

DETAILED LANDSCAPE PLANS

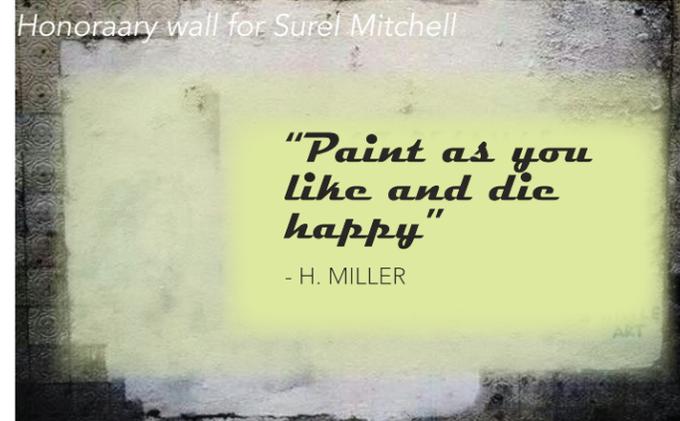
ART BLOCKS

The Art Blocks emulate a whimsical private-public interface where Live/Work community engagement is encouraged. Areas adjacent to the sidewalk tie the streetscape into the lots, allowing comfort and connection for passersby. Open flowy walkways between the units entice residents to pass through and take advantage of a couple designated shared seating areas that encourage neighborhood interaction. There are ample vegetation areas that will soften and screen the seating areas for comfort and enjoyment.

There will also be an Exhibition House that will provide opportunities for local artists to showcase and present their art at different times. It is also important to celebrate the preceding efforts to enhance Garden City, therefore a quote by historic artist Henry Miller will be added to onto a permanent art feature that honors Surel Mitchell and all artists in Garden City .



Seating area



Art feature



Sandstone bench



Artful, custom bike racks



Art exhibition house



Small patio



Firepit



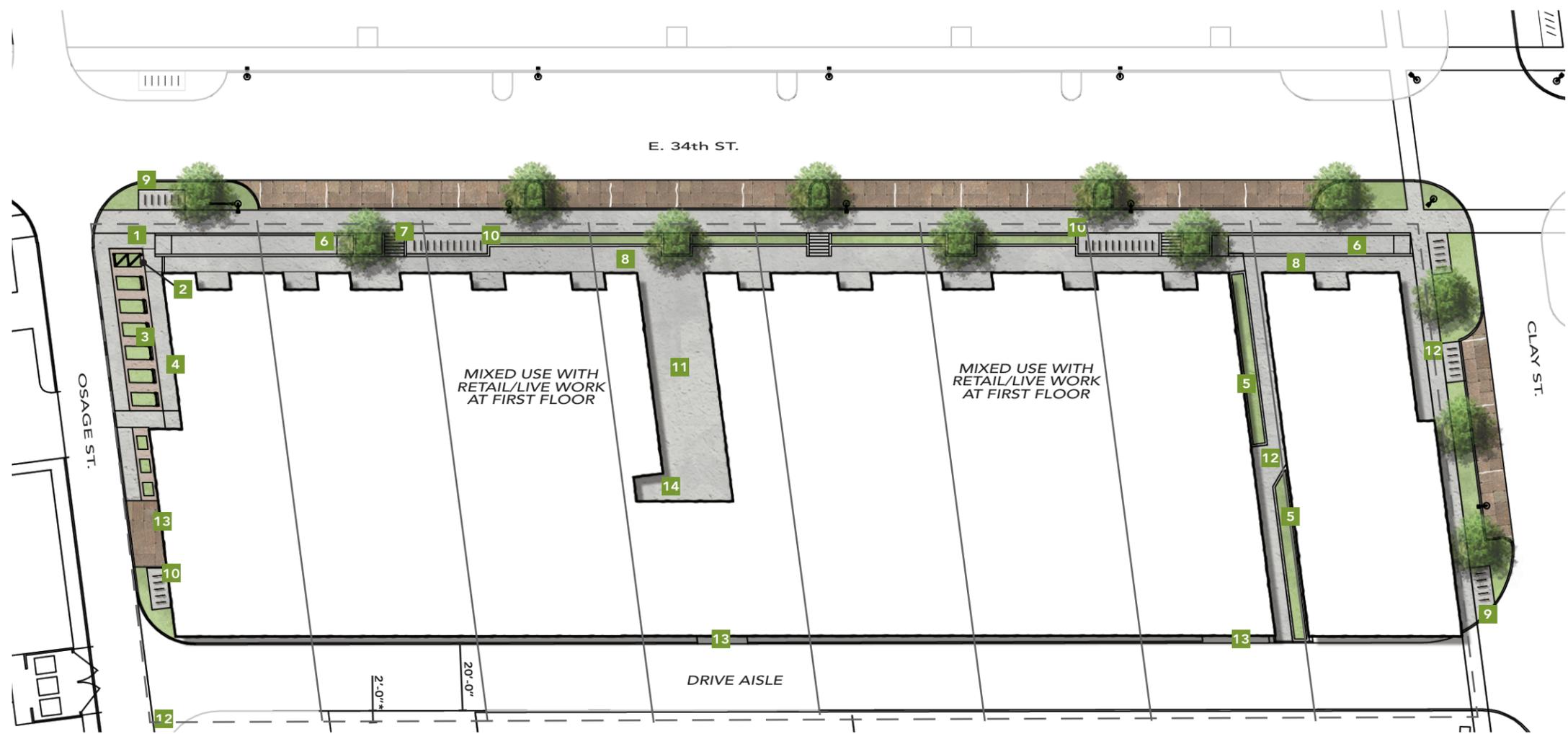
Stepping stones with water-wise plantings



Stepping stones

**Key**

- 1 Sidewalk
- 2 Interpretive signage
- 3 Steel raised garden beds
- 4 Trellis with vines
- 5 Half Bocce court
- 6 Ramp
- 7 Steps, Typ
- 8 Raised terrace at building frontage
- 9 Bike Racks
- 10 Bike rack inset at sidewalk level
- 11 Corridor
- 12 Property Line
- 13 Parking entrance
- 14 Residential building entrance for residential above



**WIREBIRD LOTS CONCEPT**



Plan 1" = 40'-0"  
 0 20' 40' 80'  
 \* Noted setbacks and dimensions are the preferred dimensions that will be drawn in Construction Documents

## DETAILED LANDSCAPE PLANS WIREBIRD LOTS

Amenities are essential to the quality of life for multi-family developments. Garden City promotes and requires amenities of different types and quantities throughout different sizes of developments.

Per Garden City's Dwelling Unit, Multi-Family (8-2C-15), the requirements are provided below.

All Multi-family developments shall provide for quality of life, open space and recreation amenities to meet the particular needs of the residents as follows:

- Types of Amenities:
  - Quality of Life Amenity: clubhouse, fitness facility, enclosed bike storage, public art
  - Open Space: grassy area 50'x100' minimum, community garden, ponds or water features, plaza
  - Recreation amenities: pool, walking trails, children's play structures, sports court
- The number of amenities required shall depend on the size of the development:  
Multi-family with 20 units or less: (2) Amenities, (1) from two of the categories

The 34th Street Specific Area Plan mixed-use development area located along the East side of 34th Street, known as the Wirebird Lots, provides 20 units and meets and exceeds the minimum amenities for this development. Proposed here are two Bocce Ball courts and Community Gardens. Further this development offers an elevated building frontage walk that features many spaces for users to sit and gather as highlighted below.

The Wirebird development offers a strongly defined building frontage space, elevated from the streetscape and sidewalk, allowing gathering and circulation up above with ease of access from below. Interpretive signage features throughout will showcase the Garden City history and it is importance to the current and future growth and identity. With easy access via stairs and ramps, enhanced visually by planters and art between, users can carry their experience up elevated building frontage spaces along 34th Street. Here there will be many streetscape activated building frontages with gathering spaces outside. Users can enjoy the goods and services offered with a prospective view over the street with easy spaces to sit and pass through. A community garden that will be used and tended by the Marketplace will showcase the importance of growing food, and taking a zero-waste approach, this promotes Garden City's history and allows the community to celebrate some of what makes it unique. Two bocce ball courts will be provided for the residents to use with each other in their rear yards.



Steel trellis for vines



Steel raised planters



Interpretive signage



Raised planters



Trellis at building



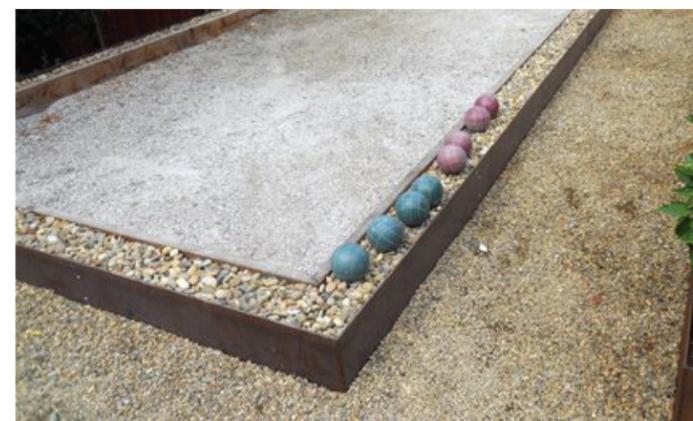
Interpretive signage



Interpretive signage



Raised building frontage



Corten Steel edge with pebbles



**DETAILED LANDSCAPE PLANS  
MARKETPLACE**

The Marketplace and plaza serves as an artistic gateway to the 34th Street Specific Area Plan with a prominent entry monument and other elements. The unique culture and mission that Roots provides is emulated outside as strongly as inside the building. Sculptural art, vines, ground level and raised planters and other aesthetic elements enhance the threshold experience. Bike racks and ample parking in addition to easy walking provide users easy access to the plaza, the benches, seating area and fire pit areas that allow spaces for users to pass time with friends and loved ones. Some of the parking will also be allowed for markets and events at the plaza, which tie one end of 34th to the other. The Marketplace will also directly benefit to the adjacent Wirebird lots that offer a community garden and similar sculptural art. The Chinden Boulevard side will be subject to recommendations per Idaho Transportation Department.



*Public art*



*Steel planter with low evergreen hedge*



*Paver plaza area for gathering & dining*



*Public art plinth element*



*Public art plinth element*



*Compost area screen fence*



*Raised planters with vegetables*



*Trellis with vines*



*Artful, custom bike racks*

**BLOCKS 30 & 35**  
**RIVER LOTS**

34th Street Specific Area Plan properties from N. Carr Street to the Boise River are located within the Boise River and Greenbelt Overlay District for the District's purpose 'to preserve, protect and enhance the river, and the public's enjoyment and access to the Boise River and Greenbelt through carefully planned and designed development. Mixed use buildings, commercial development, and urban density housing are encouraged that focus on, celebrate, connect and enhance the Boise River environment.'

This Plan supports these objectives of the Boise River and Greenbelt Overlay District:

1. To protect public access and enjoyment of the river and greenbelt through regulations on the location, orientation, density, and appearance of development.

The Specific Area Plan achieves this by opening the river side of the development to public access through public pedestrian and bicycle access easements through vacated portion of E. 34th St at N. Carr St with plaza areas. River oriented and activated buildings are behind the plaza with retail/restaurant uses on the ground floors and high density residential on the upper floors.

The Summary Chart defines specific density, location, and orientation. Other pages in this document define in more detail the plaza, landscape design and building design guidelines to support public access areas and enjoyment.

2. To encourage the highest best use development adjacent to the urban river setting through specific use standards.

The highest best use development at this intensely active urban river location is high density, mixed-use buildings with river-activated uses such as retail, restaurants, plaza and public gathering places, events and activities. See Detailed Landscape Plan for Plaza public areas.

3. To protect critical wildlife habitat and wetlands along the river through limiting development in these areas.

There are no areas of critical wildlife habitat along

the inboard side of the greenbelt path.  
4. To minimize disruption and alteration of existing waterways through limitations on grading and land clearing.

As desired by the irrigation companies, the existing ditches in this area will be re-routed and piped for public safety.

5. To enhance the natural environment with requirements for appropriate native landscaping, planting and landscape maintenance techniques.

Appropriate native landscaping and maintenance will be located at the inboard side of the greenbelt path, transitioning to more urban plantings towards N. Carr Street. See Detailed Landscape Plans. There is no proposal to change or modify the river side of the greenbelt.

6. To protect and enhance connectivity along the greenbelt and linkages with other parks, paths and green spaces through incentives that encourages dedication or easements for public use.

Connectivity to the Greenbelt will be provided through public easements

including a 50' wide pedestrian, cycle and emergency access at a vacated 34th Street, which will become a public access corridor and plaza; a 10' wide bicycle path at the south drive aisle will connect N. Carr St. to the greenbelt.

7. To create nodes of urban development in certain locations in the District through appropriate use and development standards.

This Specific Area Plan creates an exciting, vibrant, urban nodal development at a very active location on the Boise River with public plaza surrounded by mixed use buildings. Development standard chart is alongside the site plan and additional design guidelines are within the SAP.

8. To provide for emergency and maintenance services to the Boise River and Greenbelt through the requirements for public access.

This Specific Area Plan provides emergency and maintenance services to the Boise River and Greenbelt via public easements through the center plaza area.

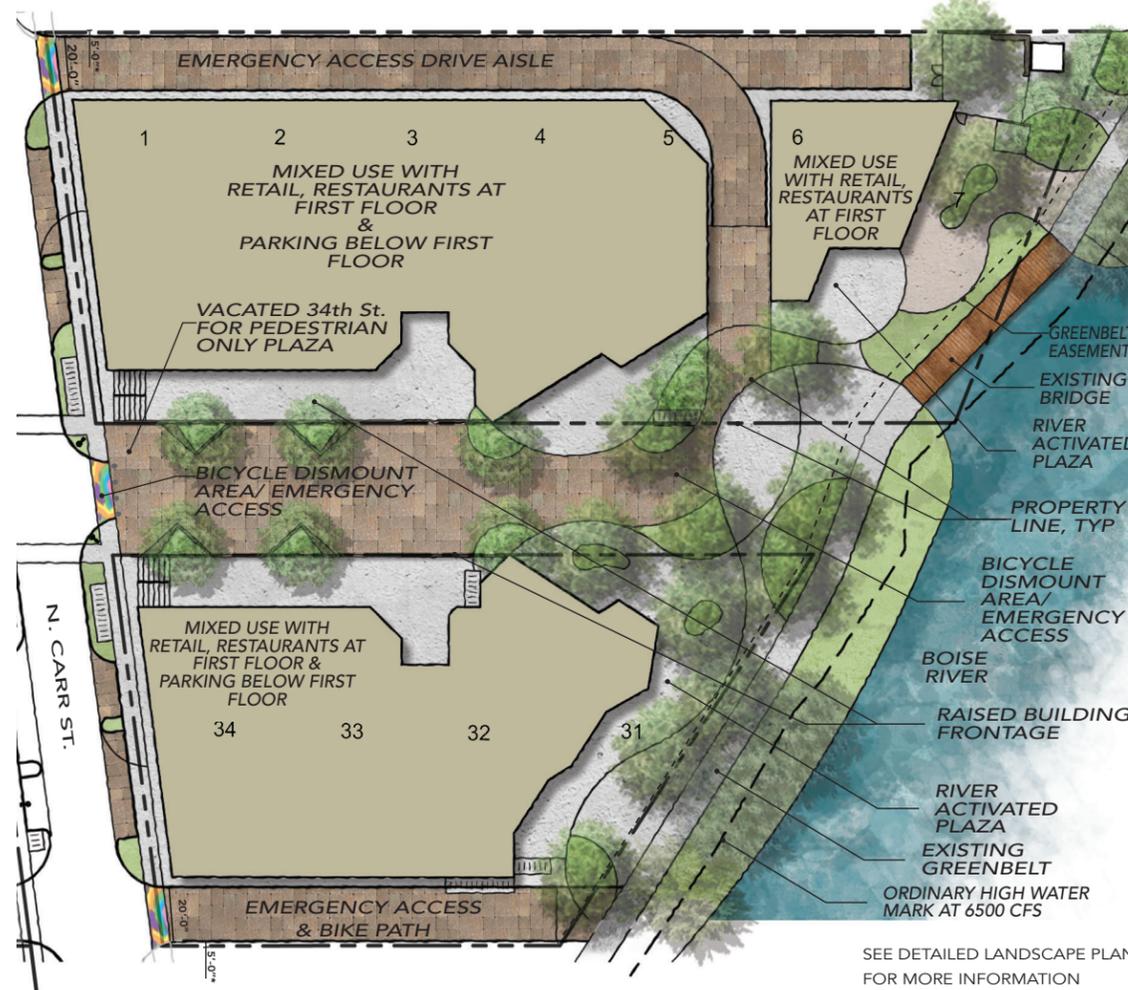


FIGURE : SITE PLAN - RIVER LOTS- BLOCKS 30 & 34



\* Noted site plan setbacks and dimensions are the preferred dimensions that will be drawn in Construction Documents

# Parking stall counts

BLOCK 35, Lots 31-34		SUMMARY	
Acreage	0.72 acres		
Dwelling Units (du)	48		
Density	66.7du/acre		
Commercial Space	16,000sf		
Residential On-Site Parking Spaces Rear or in Building	48		
On-Site Residential Parking Ratio	1:1 spaces/du		
Commercial Parking Spaces Rear or in Building	12		
On-Site Parking Total	60		
On-Street Parking	1		
ADA Parking Spaces	3		
Bike Parking Inside	20		
Bike Parking at Plaza	36		
Bike Parking at Carr St	11		
Building Entrance Orientation	Entrances Face Common Walk, Plaza, or Street		
Front Setback From Back of Sidewalk at Street	0'		
Front Setback at Common Area	0'		
Interior Side Setback	20'		
Setback to Greenbelt	53'		
Parking Entrances	From Carr St		
Buffer to Adjacent Properties to East	4' Planting buffer w/ 6' solid fence		
Maximum Stories	2 stories 10'-19' from Greenbelt; 3 stories between 20'-29' from Greenbelt; 4 stories 30' - 40' from Greenbelt; 5 stories beyond 40' from Greenbelt		
Private Open Space per unit	24sf min. porch or deck		
First Floor Above BFE	1' or more		
Garage Floor Level Above BFE	N/A		
Building	These items will be completed		
Landscaping	in future plans		
TOTAL	31,321sf		

\* Exterior stairs are allowed to project into setbacks

BLOCK 30, Lots 1-7		SUMMARY	
Acreage	1.26 acres		
Dwelling Units (du)	53		
Density	42.1 du/acre		
Commercial Space	20,200sf		
Residential On-Site Parking Spaces Rear or in Building	54		
On-Site Residential Parking Ratio	1.02:1 spaces/du		
Commercial Parking Spaces Rear or in Building	26		
On-Site Parking Total	80		
On-Street Parking	1		
ADA Parking Spaces	4		
Bike Parking Inside	80		
Bike Parking at Plaza	13		
Bike Parking at Carr St	11		
Building Entrance Orientation	Entrances Face Common Walk, Plaza, or Street		
Front Setback From Back of Sidewalk at Street	0'		
Front Setback at Common Area	0'		
Interior Side Setback	20'		
Setback to Greenbelt	62.5' Minimum		
Parking Entrances	From Carr St		
Buffer to Adjacent Properties to West	4' Planting buffer w/ 6' solid fence		
Maximum Stories Lots 1-5	2 stories between 10'-19' from Greenbelt; 3 stories between 20'-29' from Greenbelt;		
Maximum Stories Lots 6-7	3		
Private Open Space per unit	24sf min. porch or deck		
First Floor Above BFE	1' or more		
Garage Floor Level Above BFE	N/A		
Building	These items will be		
Landscaping	completed in future plans		
TOTAL	54,981sf		

\* Exterior stairs are allowed to project into setbacks



BLOCK 31, Lots 4-7, 27, 28	SUMMARY
Acreage	1.04 acre
Dwelling Units (du)	19
Density	18.3du/acre
Residential Size Range	600 - 2200sf
On-Site Parking Ratio	1:1 spaces/du
On-Site Parking Spaces in Lots	16
On-Site Parking in Garages	11
On-Site Parking Total	27
On-Street Parking	14
ADA Parking Spaces	2
On-Site Bike Parking	24
Bike Parking in Garages	8
Building Orientation at Streets	Main Entrance Face Street
Building Orientation at Common Walk	Entrance Face Common Walk
Front Setback From Back of Sidewalk at Street	5' Minimum*
Front Setback at Common Area	0*
Side Street Setback From Back of Sidewalk at Street	5' Minimum*
	0' between attached townhomes or 6' Minimum between end townhomes/detached homes
Interior Side Setback	townhomes/detached homes
Side Setback to Parking Lot Sidewalk	5' Minimum
Rear Setback to Building	3' Minimum
Parking Orientation	Rear Drive Aisle
Rear Setback Parking Space to Drive Aisle	2' Minimum (22' clear backup space to far side of drive aisle)
Side Setback at Garage	3' Minimum
Buffer Drive Aisle to Adjacent Properties	2' Planting buffer w/ 6' solid fence
Maximum Stories	3
Building Height	45' Maximum
Private Open Space per unit	24sft min. porch or deck
First Floor Living Level Above BFE	1' or more
Garage Floor Level Above BFE	N/A
Building	These items will be
Landscaping	completed in future plans
TOTAL	45,347sf

\* Exterior stairs are allowed to project into setbacks



FIGURE: SITE PLAN-BLOCK 31, LOTS 4-7, 27, 28

Plan  
NORTH

0 20' 40' 60' 80' 120'

\* Noted site plan setbacks and dimensions are the preferred dimensions that will be drawn in Construction Documents

# Parking stall counts

### BLOCK 31, LOTS 4-7, 27,28

The Yardbird lots is composed of two groupings of Live/Work/Townhomes Homes with common areas and walkways that create a sense of place and creativity for residents. Homes facing the streets will have front doors and porches and addresses that face the streets. All other homes will have their front doors, porches/patios facing the common areas. Parking is located off a newly created drive aisle and will serve as service access, solid waste pick up, and storm water retention. The common areas and drive aisles are described in more detail in the Detailed Landscape Plans.



**BLOCK 34, LOTS 8, 27-30**

The configuration of this area has two types of buildings:

Lots 8, 27, and 28 have multi-story mixed use buildings built to the back of sidewalk and facing the East/West streets. To be out of the Flood Plain, there will be raised entrance terraces accessed by stairs and ramps for dining, product sales and other activities contributing to active street life.

Lots 29 and 30 are a community of Live/Work townhomes with a central common courtyard area. The homes facing 34th Street will have entrances with raised porches and front doors facing the street. Courtyard homes behind will enter off the common courtyard.

Parking is located in garages off a newly created drive aisle and will serve as service access, solid waste pick up, and storm water retention. The common areas and drive aisles are described in more detail in the Detailed Landscape Plans.



**FIGURE : SITE PLAN - BLOCK 34, LOTS 8, 27-30**

BLOCK 34, Lots 8, 27, 28	SUMMARY
Lots 8, 27, 28 Acreage	0.51
Dwelling Units	0
Commercial Space	27,000sf
On-Site Parking Spaces Rear or in Building	12
On-Site Parking Total	11
On-Street Parking	13.5
ADA Parking Spaces	1
Bike Parking Inside	11
Bike Parking at Streets	29
Parking Orientation	Rear Drive Aisle
Building Entrance Orientation	Face Street
Front Setback From Back of Sidewalk at Street	0'
Interior Side Setback at Lot 8	5'
Side Street Setback from Back of Sidewalk	0'
Interior Side Setback at Lots 27, 28	0'
Rear Setback to Drive Aisle	2'
Maximum Stories	2
First Floor Above BFE	1' or more
Garage Floor Level Above BFE	N/A
Building	These items will be completed
Landscaping	in future plans
TOTAL	22,684sf

\* Exterior stairs are allowed to project into setbacks/common area

BLOCK 34, Lots 29, 30	SUMMARY
Acreage	0.34 acre
Dwelling Units (du)	6
Density	17.6du/acre
Residential On-Site Parking Spaces Rear or in Garages	6 in garages
On-Site Residential Parking Ratio	1:1 spaces/du
On-Street Parking	2.5
ADA Parking Spaces	0
Biking Parking in Garages	6
Biking Parking on Site	13
Bike Parking at Street	5
Building Orientation at Streets	Homes at Street Face Street
Building Orientation at Common Walk	Homes at Common area Face Common Walk
Front Setback From Back of Sidewalk at Street	0' Minimum*
Front Setback at Common Area	0**
Setback to Drive Aisle	8' Minimum*
Interior Side Setback	0' Min. between attached townhomes
Interior Side Setback to Lot 28	3' Minimum
Rear Setback to Building	0' Min. at Common Area/ 2' Garage to Drive Aisle
Parking Orientation	Rear and Side Drive Aisle
Rear Setback Parking Space/Garage to Drive Aisle	2' Minimum (22' clear backup space to far side of drive aisle)
Side Setback Garage to Lot 28	3' Minimum
Maximum Stories	3
First Floor Living Level Above BFE	1' or more
Garage Floor Level Above BFE	N/A
Building	These items will be completed
Landscaping	in future plans
TOTAL	15,090sf

\* Exterior stairs are allowed to project into setbacks/common area





BLOCK 33, Lots 5, 8-11		SUMMARY
Acreeage	0.87 acre	
Dwelling Units (du)	19	
Density	21.8du/acre	
Residential Size Range	600 - 2200sf	
Commercial Space	1000sf	
Residential On-Site Parking	19	
On-Site Residential Parking Ratio	1:1spaces/du	
On-Site Commercial Parking Spaces	4	
On-Site Parking Total	24	
On-Street Parking	16	
ADA Parking Spaces	2	
On-Site Bike Parking	35	
Building Orientation at Streets	Main Entrance Face Street	
Building Orientation at Common Walk	Entrance Face Common Walk	
Front Setback From Back of Sidewalk at Street	5' Minimum*	
Area	0*	
Side Street Setback From Back of Sidewalk at Street	5' Minimum*	
Interior Side Setback	0' between attached townhomes or 6' Minimum between townhomes/detached homes	
Side Setback to Parking Lot Sidewalk	5' Minimum	
Rear Setback to Building Parking Orientation	3' Minimum Rear Drive Aisle	
Rear Setback Parking Space to Drive Aisle	2' Minimum (22' clear backup space to far side of drive aisle)	
Side Setback End Parking Space to Lots, 4, 6, 7	3' Minimum	
Side Setback End Parking Space to Public Sidewalk to Lot 11	3' Minimum	
Maximum Stories	3	
Building Height	45' Maximum	
Private Open Space per unit	24sft min. porch or deck	
First Floor Living Level Above BFE	1' or more	
Garage Floor Level Above BFE	N/A	
Building	These items will be completed	
Landscaping	in future plans	
TOTAL	37,780sf	

\* Exterior stairs are allowed to project into setbacks

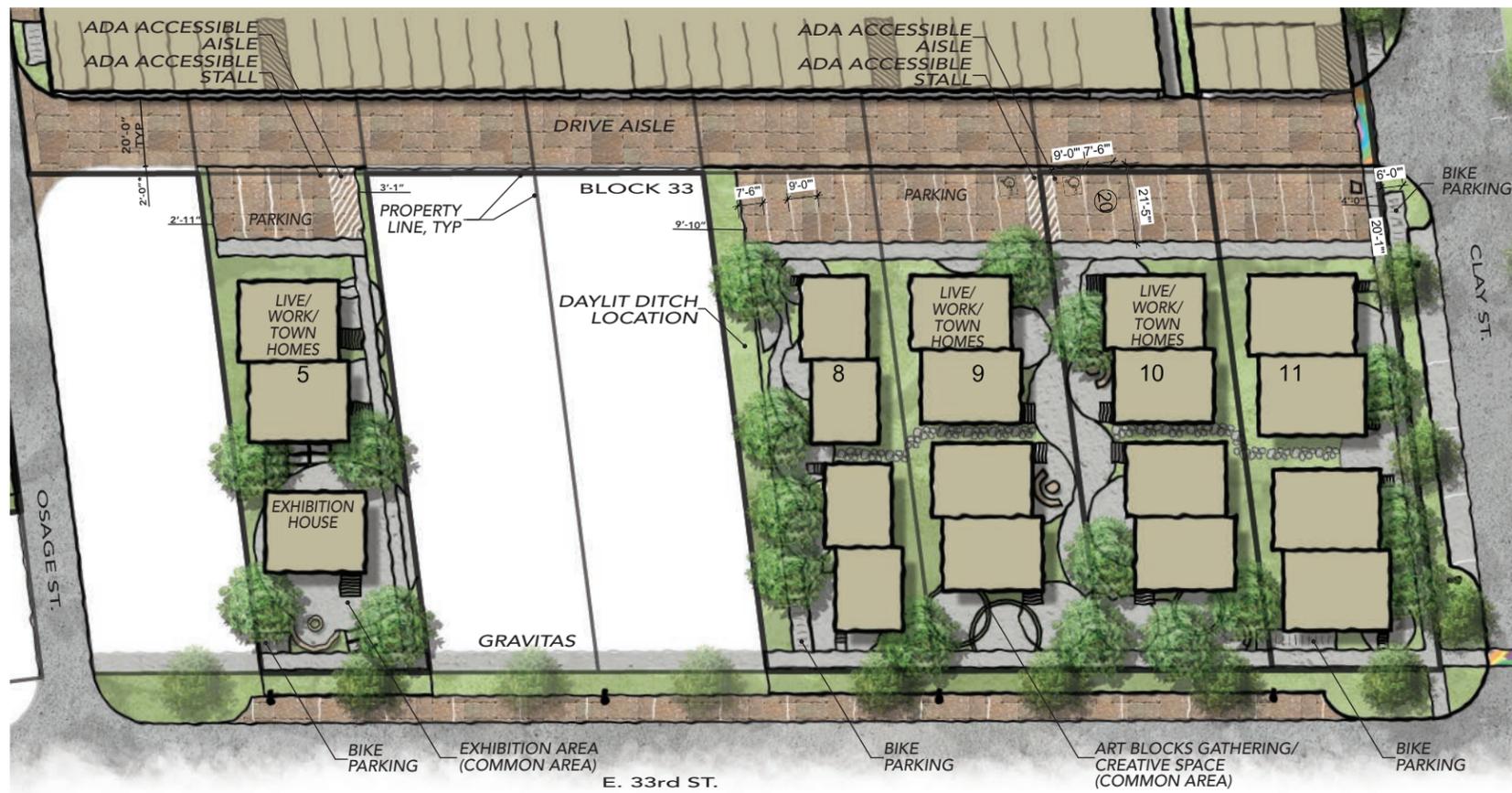
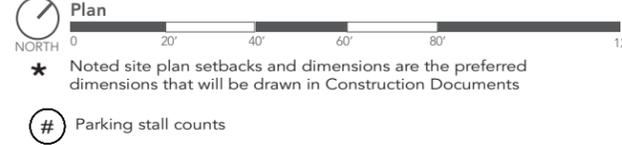


FIGURE : BLOCK PLAN - BLOCK 33, LOTS 5, 8-11



### BLOCK 33, LOTS 5, 8-11 Art Blocks

The Live/Work/Townhomes in this area are part of the 'Art Blocks' on E. 33rd Street near where the Surel's Place for artist-in-residence was initiated and remains key to the art community. The exhibition House will compliment Surel's Pace, which currently anchors the river end of 33rd Street. The homes will either face the street or face common areas and walkways that create a sense of place and creativity for residents. Two interactive creative/exhibit spaces along 33rd Street will engage the artists with the public. Homes facing the streets will have front doors and porches and addresses that face the streets. All other homes will have their front doors, porches/patios facing the common areas. Parking is located off a newly created drive aisle and will serve as service access, solid waste pick up, and storm water retention. The common areas and drive aisles are described in more detail in the Detailed Landscape Plans.



### BLOCK 33, LOTS 24-31

Multi-story mixed use buildings built to the back of sidewalk and facing 34th Street will have Live/Work, retail and/or restaurants to support a lively street environment. To be out of the Flood Plain, the terraces will be raised and accessed by stairs and ramps for dining, product sales and other activities. Balconies, terraces, and roof decks/gardens for the residences above will also enliven the facades and engage the street.

Parking is located in garages off a newly created drive aisle and will serve as service access, solid waste pick up, and storm water retention. The common areas and drive aisles are described in more detail in the Detailed Landscape Plans.

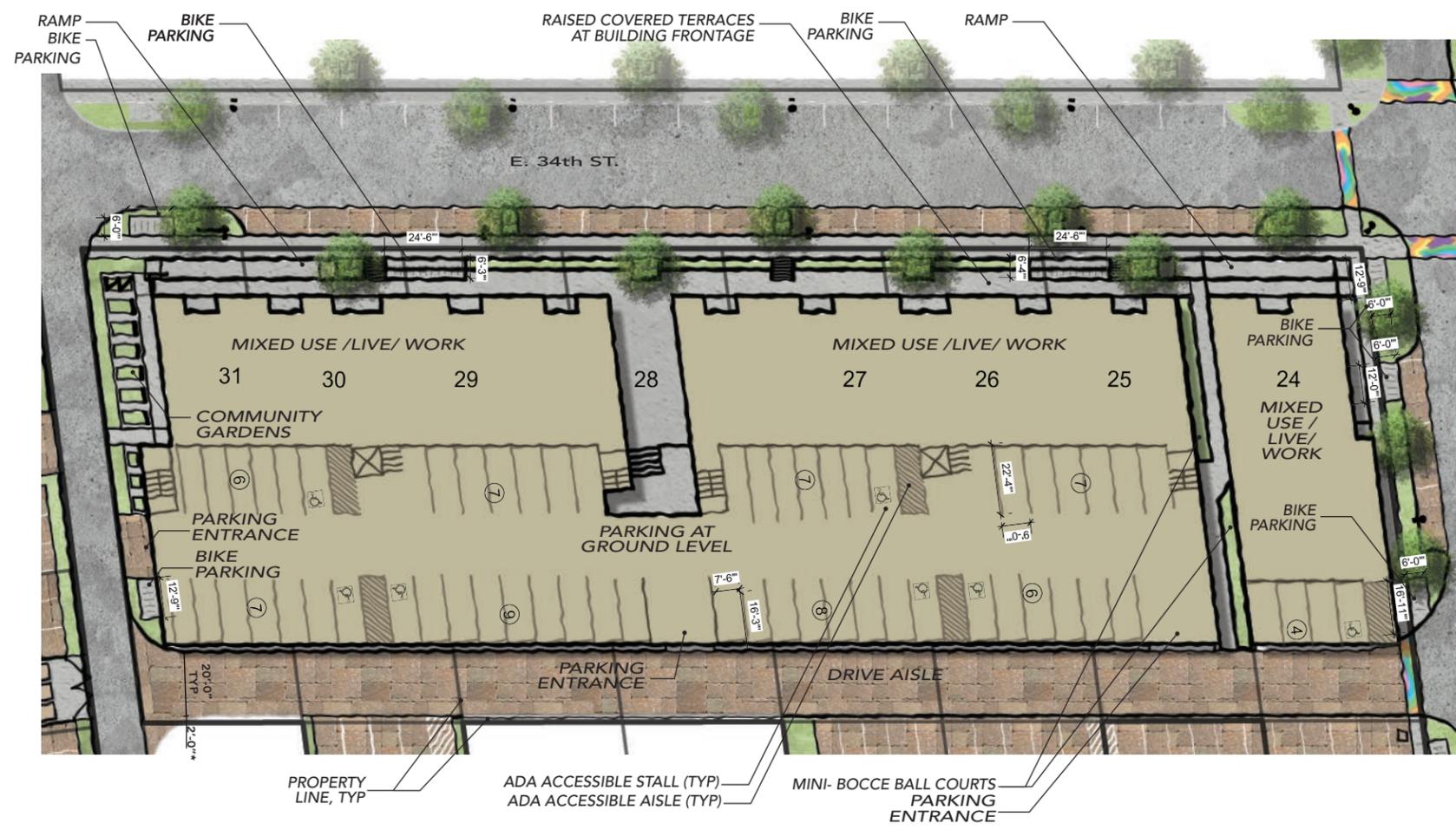
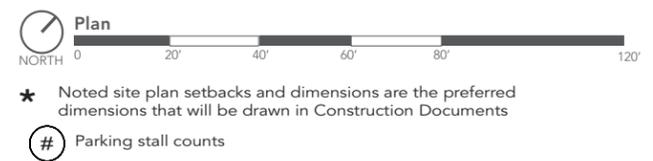


FIGURE: SITE PLAN- BLOCK 33, LOTS 24-31



BLOCK 33, Lot 24	SUMMARY
Lots 24 Acreage	0.17
Dwelling Units	0
Commercial Space	12,000sf
On-Site Parking Total	4
On-Street Parking	4
ADA Parking Spaces	1
On-site Bike Parking	2
Bike Parking at Streets	18
Building Entrance Orientation	Face Street
Front Setback From Back of Sidewalk at Street	0'
Side Street Setback from Back of Sidewalk	0'
Interior Side Setback at Lot 25	0'
Parking Orientation	Rear Drive Aisle
Rear Setback to Drive Aisle	2' Minimum
Buffer Drive Aisle to Adjacent Property to East	2' Planting buffer w/ 6' solid fence
Maximum Stories	4
Private Open Space per unit	24sft min. porch or deck
First Floor Above BFE	1' or more
Garage Floor Level Above BFE	N/A
Building Landscaping	
TOTAL	7,550sf

BLOCK 33, Lots 25-31	SUMMARY
Acreage	1.22 acres
Dwelling Units	57
Dwelling Units/Acre	46.7du/ac
Commercial Space	12,000sf
Residential On-Site Parking Spaces	57
On-Site Parking Ratio	1:1 spaces/du
On-Site Parking Total	57
On-Street Parking	14
ADA Parking Spaces	4
Bike Parking Interior	10
Bike Parking at 34th Street	37
Building Entrance Orientation	Face Street
Front Setback From Back of Sidewalk at Street	0'
Interior Side Setback to Lot 24	10'
Interior Side Setback Between Buildings on Lots 25-31	0'
Side Street Setback from Back of Curb at Osage St	10'
Rear Drive Aisle/Osage Street	
Parking Orientation	2' Minimum
Setback to Drive Aisle	
Buffer Drive Aisle to Adjacent Property to East	2' Planting buffer w/ 6' solid fence
Maximum Stories	3-4
Private Open Space per unit	24sft min. porch or deck
First Floor Above BFE	1' or more
Garage Floor Level Above BFE	N/A
Building Landscaping	<i>These items will be completed in future plans</i>
TOTAL	52,954sf



BLOCK 33, Lts 2,3,32-34	SUMMARY
Acreage	0.86
Dwelling Units	0
Commercial Space	30,000sf
On-Site Parking Spaces	42
On-Site Parking Total	42
On-Street Parking	3
ADA Parking Spaces	2
Bike Parking at Front	20
Bike Parking at Rear	10
Parking Orientation	Osage Street
Building Entrance Orientation	Face Street
Front Setback From Property Line	5'
Side Street Setback from Back of Sidewalk	5'
Interior Side Setback	5'
Rear Setback to Drive Aisle	5'
Buffer to Adjacent Property to East	5' Planting buffer w/ 6' solid fence
Maximum Stories	2
Usable Common Area	
First Floor Above BFE	1' or more
Garage Floor Level Above BFE	N/A
Building	These items will be completed
Landscaping	in future plans
TOTAL	37,500sf

\* Exterior stairs are allowed to project into setbacks/common area



FIGURE : BLOCK PLAN - BLOCK 33, LOTS 2,3,32-34



\* Noted site plan setbacks and dimensions are the preferred dimensions that will be drawn in Construction Documents # Parking stall counts

## BLOCK 33, LOTS 2,3,32-34

Intended as a 'gateway building' to the area, and fronting Chinden Boulevard, is a large single story articulated building, with a single corner entrance at E. 34th Street or multiple entrances facing Chinden Boulevard and E. 34th Street. 'Green Boulevard' plantings and raised bed gardens will be installed along Chinden Boulevard, and a plaza and event space with gardens along E. 34th Street. Parking is located at the rear accessed of Osage Street.

The common areas, parking, and public art are described in more detail in the Detailed Landscape Plans.



### BLOCK 31, LOTS 1,2 Ultimate Parklet

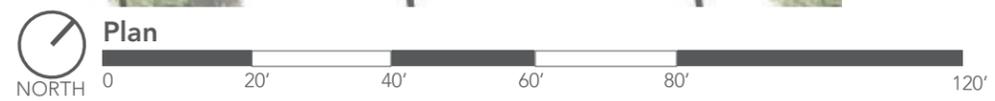
The Ultimate Parklet is a dynamic anchor space that serves community life and functional needs for visitors. It serves as a welcome area for visitors and attraction for those coming up 34th from Chinden, and a community gathering space that offers various amenities. This space challenges the ordinary by creating a sense of place within something that is typically functional, parking. The parklet is transformative in nature, often will be converted to a 'park' where parking is given back to the people for opportunities such as community gatherings and events. Users can take advantage of shaded seating areas, vendors, a mobile library, pet park, showers and drinking fountains, surf-board storage and bicycle parking. A parking space at Clay Street is designated for Uber & Lyft drivers to park in when they're ready to pick up riders. This space emulates all that the 34th Street Specific Area Plan envisions to achieve, making places better, adaptable & flexible, community oriented with an inviting appeal for others.



FIGURE: SITE PLAN- BLOCK 31, LOTS 1,2

BLOCK 31, Lots 1,2	SUMMARY
Acreage	0.34
Dwelling Units	0
Commercial Space	200sf
On-Site Parking Total	24
On-Street Parking	6
ADA Parking Spaces	1
Bike Parking	17
Parking Lot Entry Orientation	Clay Street
Building Entrance Orientation	Face Parklet
Front Setback From Back of Sidewalk at Street	5'
Interior Side Setback to Lot 3	5'
Side Street Setback from Back of Sidewalk	0' min.
Interior Side Setback to Lots 33,34	5' min.
Maximum Stories	1
First Floor Above BFE	0'
Garage Floor Level Above BFE	N/A
Building	These items will be completed
Landscaping	in future plans
TOTAL	15,110sf

\* Exterior stairs are allowed to project into setbacks/common area



\* Noted site plan setbacks and dimensions are the preferred dimensions that will be drawn in Construction Documents  
 # Parking stall counts

USE	BLK	Stories
Mixed Use	30, Lots 1-5	4 -5
Mixed Use	30, Lots 6,7	2 -3
Med Density Multifamily	31, Lots 4-8, 27, 28	2 -3
Parklet and Parking	31, Lots 1,2	1
Commercial	33, Lots 2,3,32-34	2
Med Density Multifamily	33, Lots 5, 8-11	2 -3
Mixed Use	33, Lots 25-31	3 -4
Mixed Use	33, Lot 24	3 -4
Mixed Use	34, Lot 8	2-3
Mixed Use	34, Lot 27	2-3
Mixed Use	34, Lot 28	2-3
Med Density Multifamily	34, Lots 29, 30	2-3
Mixed Use	35	4 -5



FIGURE : BUILDING HEIGHTS

**BUILDING HEIGHTS**

Maximum story heights are depicted on the map and images opposite. The intention is to create an intimate urban feeling along 34th Street by siting buildings close together and close to the street, similar to traditional downtown areas. This pattern would continue down to the river where they would widen out to the plaza and views. Buildings along 34th Street are 2-4 stories, while buildings on 33rd and 35th are maximum 2 stories except at Carr Street. The river-activated larger buildings will be 4-5 stories maximum and step back from the river, with balconies, terraces and roof top common areas.

**Legend**

- ● ● ● BIKE & PEDESTRIAN GREENBELT
- 1 STORY
- 2 STORIES
- 2 - 3 STORIES
- 3-4 STORIES
- 4-5 STORIES



## BUILDING DESIGN STANDARDS

Purpose: The purposes of the following building design standards are to create:

- a. Urban style buildings with massing up to the street and intersections, that include human scale elements.
- b. Designs that create a sense of place and setting for pedestrian activity and mobility;
- c. Designs with a seamless interface with the street, sidewalk and public spaces;
- d. Exteriors that reflect the eclectic light industrial heritage of Garden City, while using enduring materials.

The images are conceptual examples of building materials and character. The detailed building design will be established at the time of construction drawings.

- 1. The configuration and scale of buildings shall be designed to interact with the greenbelt, open spaces and gathering places to create a dynamic and interesting environment.
- 2. Stand-alone building styles shall be discouraged.
- 3. Buildings facing the greenbelt and river should present an undulating front to the greenbelt so that a large expanse of a building does not dominate the greenbelt and river. This should be accomplished through the use of form changes with setbacks/stepbacks. Additional building design components can contribute to the undulation such as balconies, bay windows, canopies, awnings, window shading devices, change of materials or colors, porches and outdoor spaces.
- 4. All residential uses shall provide space in addition to enclosed garages for the storage of recreational and other equipment.



5. Building design for all uses are encouraged to include spaces that support bicycle use including: enclosed bicycle storage with easy connection to the greenbelt and other bicycle trails; cleaning and maintenance facilities; and showers and changing area for uses with employees.

6. Building designs are encouraged to provide for exterior public art on the building and within common areas.

7. Building styles should reflect the historic light industrial character of the East End and other traditional historic warehouse/light industrial areas with simple aggregated forms.

8. Facades visible from a public street shall incorporate openings and variety in the facade, including, but not limited to, the use of porches, terrace, recessed entrances, dormers, awnings, arcades, and columns.

9. Exterior building materials should be of durable and long lasting quality such as brick, concrete, stone and metal; accents may be of stucco, CMU, and wood. Materials of a less permanent nature, such as plywood and thin veneer, should not be the dominant building material. Materials of an industrial nature that are well integrated into the scale and design of building are appropriate. At least two (2) changes in one or a combination of the following shall be incorporated into the building design: color, texture and/or materials.

10. No blank walls should front a public street. Any blank walls should be treated in one or more of the following ways:

- a. Installing a vertical trellis in front of the wall with climbing vines or plant materials; or



- b. Providing a landscaped planting bed at least five feet (5') wide or raised planter bed at least two feet (2') high and three feet (3') wide in front of the wall, with plant materials that obscure or screen at least fifty percent (50%) of the wall's surface within three (3) years;
- c. Providing artwork (mosaic, mural, sculpture, relief, etc.) over at least fifty percent (50%) of the blank wall surface.

11. Windows shall total a minimum of twenty percent (20%) of the building elevation facing a public street, as measured by the area from the ground level to eighteen feet (18') above the ground of all building planes visible and parallel to the street.

12. Window styles to avoid are sliding windows of equal size sections.

13. Main Entrances should face the street or common areas and shall be adequately covered, recessed, or treated with a permanent architectural feature in such a way that weather protection is provided.

14. Storefronts and large windows are highly desirable on first floor commercial spaces.

15. Roofs may be sloped or flat; design shall demonstrate variety and interest including, but not limited to, the use of overhanging eaves, sloped roofs, and/or varying parapet heights.

16. Roof terraces and gardens are encouraged. Unsightly equipment will be screened. Furnishings should be secured from high winds.

17. All roof and wall mounted mechanical, electrical, communications, and service equipment should be screened from public view from the adjacent public streets and properties by the use of parapets, walls, fences, enclosures, or by other suitable means.





## PHASING

The phasing plan represents the intended sequence of development and will follow the development of infrastructure.

- Phase One of the development of the Specific Area Plan is the 'Yardbird' Subdivision along 34th Street, 35th Street and the adjacent N. Carr Street. The Subdivision will include Original Lots of Record, Lots 4-7, 27, 28, Block 31, and Lots 29 and 30, Block 34. Adequate sewer and water infrastructure are currently available to serve the subdivision. Joint trench utilities and gravity irrigation piping will be constructed within the boundaries of the subdivision prior to final platting. Adjacent streets will be constructed with 'half street plus 12', including streetscapes as per Ada County Highway District. It is anticipated that the sewer and water infrastructure in 34th Street between Clay Street and Carr Street will need to be upgraded at this time. The sewer will be replaced manhole to manhole.

- Phase Two is grading and building the Plaza, including moving and piping irrigation lines and adjusting sewer and water in that area. Boise City has a 48-inch sewer interceptor line which runs in Carr St. between 33rd and 34th Streets then northeast on 34th St. to the Boise River Greenbelt. The sewer interceptor will not be changed. Manholes will need to be adjusted to grade as the site develops. Other critical infrastructure for the area will be installed such as 3-phase power, the sewer reconstruction NE from Carr on 34th, and any water line upgrades necessary for fire flow.

- Phase Three is building the mixed use building on Block 30, Lots 6 and 7, and the adjacent emergency access drive with joint trench and irrigation piping.

- Phase Four is developing the Ultimate Parklet on Block 31, Lots 1 and 2 including adjacent streets with 'half street plus 12', as per Ada County Highway District, and streetscapes, and water line upgrades to Clay Street.

- Phase Five is building the 3 mixed use buildings at E. 34th Street and N. Car Streets on Block 34, Lots 8, 27, 28, including adjacent streets with 'half street plus 12', as per Ada County Highway District, and streetscapes. The 12 inch water line in Carr St. between 34th and 35th Streets will be upgraded with the development progress. Piping for the irrigation line at rear will be installed as required by the ditch companies.

- Phase Six is building the Live/Work/Townhomes on 33rd Street on Block 33, Lots 5, 8-11 including access drive aisle on the lots to the north, and adjacent streets with 'half street plus 12', as per Ada County Highway District, and streetscapes. Power and joint trench will be installed under the drive aisle. The ditch at Lot 5 will be opened up and the irrigation line at Lot 10 will be piped.

- Phase Seven is building the south side of E. 34th Street at Block 33, Lots 24-31, including adjacent streets with 'half street plus 12', as per Ada County Highway District, and streetscapes. The sewer in 34th St. will require replacement with PVC from Clay to Osage manhole to manhole. The water line may need an upgrade.

- Phase Eight is the development on W. Chinden Boulevard which would include all necessary site improvements and 'half street plus 12' on 34th Street, as per Ada County Highway District, including streetscapes.

- To be developed any time after Phase Three are the remaining river lots – Lots 1-5, Block 30, Lots 31-34, Block 35 providing drive aisle, joint trench, and adjacent streets with 'half street plus 12', as per Ada County Highway District, and streetscapes.

**NOTE:**

Street construction will be concurrent with site construction. All of the phases listed above will follow the construction of infrastructure. We are proposing urban buildings directly adjacent to the street improvements, so it will be necessary to coordinate construction such that building site construction does not undermine street improvements. The sewer line at Osage Street is a 10 inch line and flows northwesterly under Osage to 36th Street where it increases to 12 inch. We will be working with Garden City Public Works as final property uses are determined to analyze sewer requirements and sewer capacity.

## SPECIAL THANKS

Together we have accomplished something great! This is the type of project we can admire and say “we did that”!

The vision of one can not be accomplished without the work of many. I’m so very grateful to have a wonderful team of creatives working with me.

A special thank you to my boss, friend and mentor. Thank you for your patience with me and believing we can accomplish great things together in Garden City! Your wisdom and dedication to your projects is admired. You always provided the best advice and kept me laughing on tough days. I’m very thankful for the lessons you have helped me learn.

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A special thank you to my entire team, for all your patience, hard work and dedication to the bigger mission of helping Garden City!

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To all my wonderful friends that have believed and supported my dream of cleaning up Garden City! You’ve have been the most wonderful friends! There was always someone there to reinforce the dream and say “keep going” we have grown this community together and I look forward to our next adventures.

With much love and gratitude

Thank you,

Hannah Ball  
Urban Land Development

*“When I say artist I mean the one who is building things...*

*some with a brush, some with a novel,  
some choose a pen.”*

*- Jackson Pollock*

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